



City of Bothell™

**A G E N D A**

**LANDMARK PRESERVATION BOARD**

**\*\*VIRTUAL MEETING\*\***

**Regular Meeting**

**Tuesday, July 28, 2020 6:00pm**

**PUBLIC NOTICE:** Pursuant to Governor Inslee's Stay Home, Stay Healthy Proclamation, and in effort to curtail the spread of the COVID-19 virus, this Landmark Preservation Board meeting will be conducted remotely. We encourage members of the public to attend and participate in the meeting remotely, as described in more detail below.

To attend the meeting:

- Listen to the meeting live by phone: +1-510-338-9438 USA Toll / Access code: 126 628 7994
- Submit your Written Comments before 3:00 PM (day of meeting) to: [sarah.desimone@bothellwa.gov](mailto:sarah.desimone@bothellwa.gov)

**I. CALL TO ORDER**

**II. NON-AGENDA PUBLIC COMMENTS**

**III. APPROVAL OF MINUTES**

June 23, 2020 regular minutes

**IV. NEW BUSINESS**

- A.** None

**V. OLD BUSINESS**

- A.** Proposed mitigative measures for the demolition of the Ericksen House & Carriage House (Urbane Village II).

**VI. REPORTS**

**VII. REPORTS FROM STAFF**

**VIII. OPEN TOPIC DISCUSSION**

**IX. ADJOURNMENT**

# MEMORANDUM

## Community Development



City of Bothell

**DATE:** July 28, 2020

**TO:** Landmark Preservation Board

**FROM:** Sarah Desimone, Historic Preservation Consultant

**SUBJECT:** Landmark Preservation Board meeting agenda and supporting materials for June 23, 2020.

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The next LPB meeting will be held on **July 28, 2020 at 6:00pm** as scheduled. Pursuant to Governor Inslee's Stay Home, Stay Healthy Proclamation, and in effort to curtail the spread of the COVID-19 virus, the meeting will be conducted entirely remotely.

Instructions for remote attendance by LPB members:

- Click on the green "join meeting" button in your Webex meeting invitation calendar entry
- If you cannot attend via Webex, you may join the meeting live by phone: 1-510-338-9438 USA Toll/Access code: 126 628 7994; meeting password: LandmarkB2020

The LPB will be reviewing proposed mitigation measures for the Erickson House/Urbane Village II demolition. This memo transmits to the board the following information:

Att-1 Agenda for July 28, 2020 with meeting minutes from June 23, 2020

Att-2 Comments/Mitigation requirements for Urbane Village II

Att-3 Urbane Village Entry Arch proposal

Att-4 Urbane Village Linear Park Plans

Att-5 Urbane Village Historical Kiosk Plans

### **ERICKSEN HOUSE MITIGATION PROPOSAL**

#### **Purpose/Action**

The Board shall review the materials presented for concurrence with the mitigation requirements issued by the LPB on July 31, 2018 and comments made at their regular meeting on June 26, 2018. Board will make a motion to accept the proposal as presented or require modifications. Board may authorize staff to execute the final product or ask the applicant to return to a later meeting for approval.

#### **Background**

Pulte Group intends to apply for a demolition permit for the Ericksen House and Carriage House, located at 23718 Bothell Everett Highway, Bothell, 98021, in order to proceed with the Urbane Village II development. Staff determined that the buildings are eligible for listing in the Bothell Register of Historic Landmarks. City of Bothell Landmark Preservation Board (Board) held a public meeting on April 24, 2018 to negotiate alternatives to demolition with representatives from Pulte. Pulte did not provide an analysis of alternatives to demolition as required by BMC 22.28.060 so the board could not begin the 45 day negotiation period. The Board held a second public meeting

on June 26, 2018 to begin discussion of alternatives to demolition. The board held a third public meeting on July 24, 2018 where it ended the 45 day negotiation period and imposed the following mitigation requirements:

### Mitigation Requirements

The Board requires the following measures as mitigation for the loss of the Ericksen House and Carriage House:

1. The Ericksen House and Carriage House shall be offered to the public for moving and restoration with enough time given to facilitate such a project. Nine months, as suggested by applicant, will be considered to be a sufficient amount of time. Applicant shall post a notice of availability, including photographs and historical information, on the Department of Archaeology and Historic Preservation's blog, in the Bothell-Kenmore Reporter and in the Seattle Times. - *completed 7/2/19*
2. Documentation of the structure as set forth in BMC 22.28.060 A.9. - *not completed*
3. **Interpretive signage shall be placed on the site, in or near the public right-of-way, and shall include the following, at a minimum:**
  - a. Ericksen family history
  - b. Country Village history
  - c. History of other previous owners, pending findings of historical report
  - d. historic photos
  - e. map of location(s) and relationship to Bothell
4. **Board shall approve the content, design and placement of the interpretive signage.**
5. Building materials from the Ericksen House and Carriage House shall be offered for salvage by the public. Applicant shall post a notice of availability, including photographs and historical information, on the Department of Archaeology and Historic Preservation's blog, in the Bothell-Kenmore Reporter and in the Seattle Times. - *not completed*

### **Proposal**

Pulte proposes to create a linear park reaching from Bothell Everett Highway to the west end of the development and install an entry arch and a kiosk with interpretive signage at the eastern entrance of the park.



*Left: Urbane Village II development depicting the linear park*



Above left: rendering of the arch; Above right: entry arch and kiosk location

The covered, hexagonal interpretive signage kiosk will be placed near the eastern edge of the park and will feature five historical panels and a sixth panel for posting community notices. Board shall review the proposed historical panels (Att-7) for accuracy, relevancy and design as well as their placement on the kiosk, indicated by letters A through F.

Board shall also review Att-5 and Att-6 for detailed park and entry arch plans.

**Staff Recommendation**

The mitigation requirements require the inclusion of relevant family histories on the signage pending the completion of the historical report. The historical report has not been received by staff so the accuracy of the information cannot be determined. It would appear that the Greene family owned the building for 40 years and ran Greene’s Electrical from the carriage barn for 20 years. If that is accurate, the Greene family should be more prominently featured and the Ericksen family less so. Staff also notes that the Carriage Barn and the smaller building located behind it are conflated on panels E and D.

Bothell Landmark Preservation Board  
**COMMENTS AND MITIGATION REQUIREMENTS**

Subject: Comments and Mitigation Requirements for the Ericksen House and Carriage House demolition

Date: July 31, 2018

Staff Contact: Sarah Desimone, Community Development

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**HISTORY**

Pulte Group intends to apply for a demolition permit for the Ericksen House and Carriage House, located at 23718 Bothell Everett Highway, Bothell, 98021, in order to proceed with the Urbane Village II development. Staff determined that the buildings are eligible for listing in the Bothell Register of Historic Landmarks. City of Bothell Landmark Preservation Board (Board) held a public meeting on April 24, 2018 to negotiate alternatives to demolition with representatives from Pulte. Pulte did not provide an analysis of alternatives to demolition as required by BMC 22.28.060 so the board could not begin the 45 day negotiation period. The Board held a second public meeting on June 26, 2018 to begin discussion of alternatives to demolition. The board held a third public meeting on July 24, 2018 where it ended the 45 day negotiation period and imposed final mitigation requirements.

**DOCUMENTS TO BE PROVIDED TO APPLICANT**

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1. Copies of Board meeting minutes.
  2. Formal Board comments and mitigation requirements
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**LANDMARK PRESERVATION BOARD PROCEDURE**

**22.28.060 Demolition - Consideration of alternatives - Applicable process for properties on the historic register or within a district on the historic register - Applicable process for properties on the historic inventory but not on the register.**

A. Anyone applying for a demolition permit for a property on the historic register or within a district on the historic register, or for a property on the historic inventory but not on the historic register, shall prepare a report analyzing the following alternatives (listed in descending order of preference) as to their feasibility:

1. Redesigning the project to avoid any impact to the structure or its setting;
2. Incorporating the structure into the overall design of the project;
3. Converting the structure into another use (adaptive reuse);
4. Relocating the structure on the property;
5. Relocating the structure to another property within the city of Bothell or its planning area; or
6. King and Snohomish County; or
7. Washington State;
8. Salvaging from the structure historically significant architectural features and building materials;
9. Documenting the structure as a whole and its individual architectural features in photographs, drawings, and/or text. Such documentation shall be submitted to, and archived by, the city.

The report shall consist of thorough, deliberative analyses of each of the alternatives, explaining why each alternative is or is not feasible.

Landmark Preservation Board

Subject: Comments and Mitigation Requirements for Ericksen House/Carriage House Demo

Date: July 31, 2017

Staff Contact: Sarah Desimone, Community Development

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C. Any application for a demolition permit for a property that meets the criteria necessary to be listed on the local register (BMC [22.16.010](#)) shall be referred to the board. At the next regularly scheduled meeting the board shall meet with the applicant in an attempt to find alternatives to demolition of the property. These negotiations may last no longer than 45 days unless both parties agree to an extension. If no alternative to demolition can be found, the board may take up to 45 additional days to develop mitigative measures (e.g., to encourage the landowner to salvage significant architectural features of the building) and to require documentation of the building as set forth in subsection (A)(9) of this section before the demolition permit is issued.

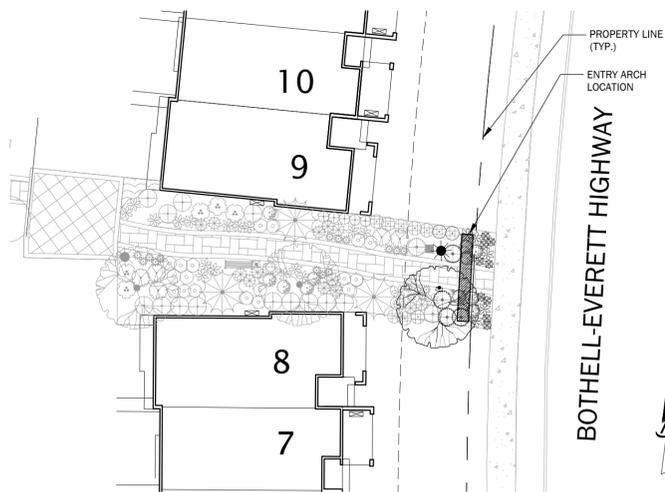
### **COMMENTS & RECOMMENDATIONS**

The Board understands that the proposed development cannot accommodate the Ericksen buildings and recommends that the Ericksen House be offered to the public to be moved to another site. The Board requests that Pulte give notice early in the process so that anyone with an interest in the buildings will have time to plan a project. The Board would like to see the Ericksen family and Country Village memorialized somewhere on the site, in or near the public right-of-way.

### **MITIGATION REQUIREMENTS**

The Board requires the following measures as mitigation for the loss of the Ericksen House and Carriage House:

1. The Ericksen House and Carriage House shall be offered to the public for moving and restoration with enough time given to facilitate such a project. Nine months, as suggested by applicant, will be considered to be a sufficient amount of time. Applicant shall post a notice of availability, including photographs and historical information, on the Department of Archaeology and Historic Preservation's blog, in the Bothell-Kenmore Reporter and in the Seattle Times.
2. Documentation of the structure as set forth in BMC 22.28.060 A.9.
3. Interpretive signage shall be placed on the site, in or near the public right-of-way, and shall include the following, at a minimum:
  - a. Ericksen family history
  - b. Country Village history
  - c. History of other previous owners, pending findings of historical report
  - d. historic photos
  - e. map of location(s) and relationship to Bothell
4. Board shall approve the content, design and placement of the interpretive signage.
5. Building materials from the Ericksen House and Carriage House shall be offered for salvage by the public. Applicant shall post a notice of availability, including photographs and historical information, on the Department of Archaeology and Historic Preservation's blog, in the Bothell-Kenmore Reporter and in the Seattle Times.



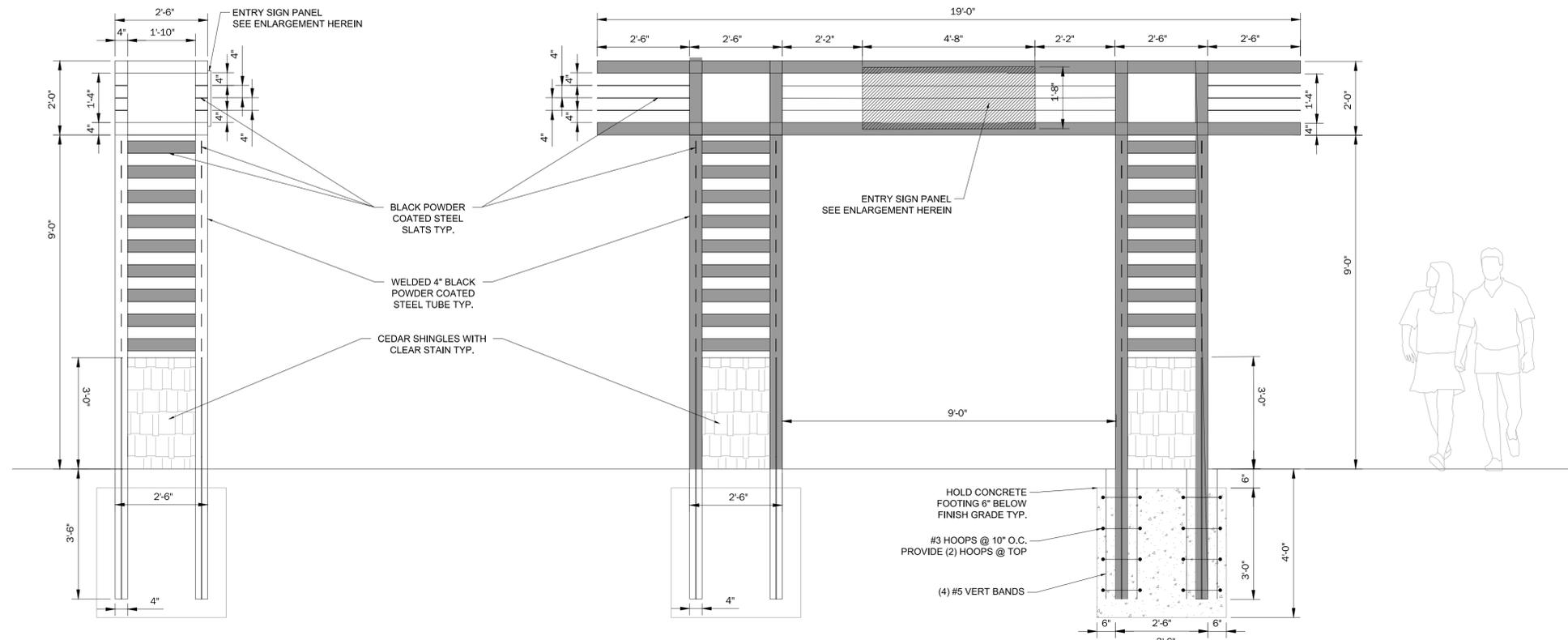
1 ENTRY ARCH LOCATION

SCALE: 1" = 20'



KEY MAP

NOT TO SCALE

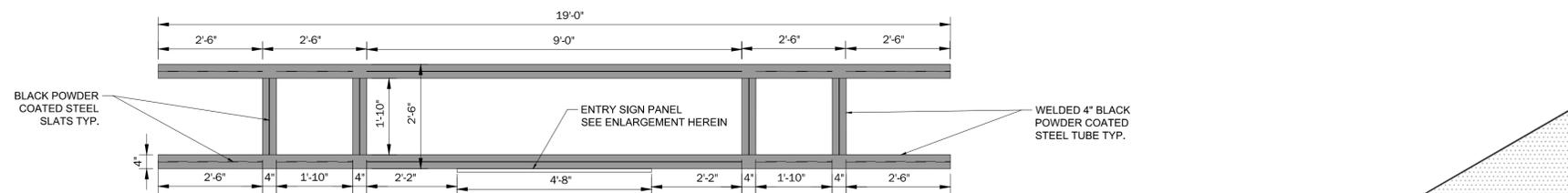


2 ENTRY ARCH SECTION VIEW

SCALE: 1" = 2'

3 ENTRY ARCH ELEVATION VIEW

SCALE: 1" = 2'



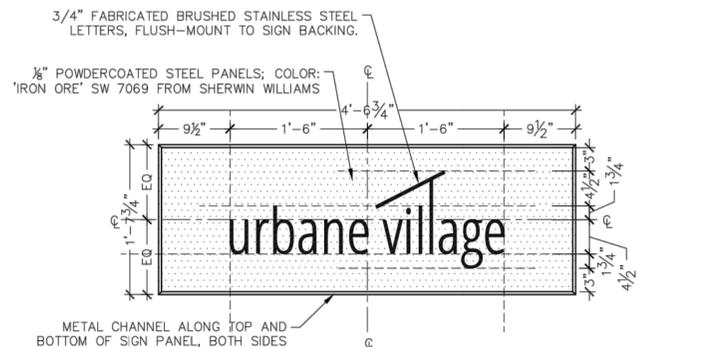
4 ENTRY ARCH PLAN VIEW

SCALE: 1" = 2'



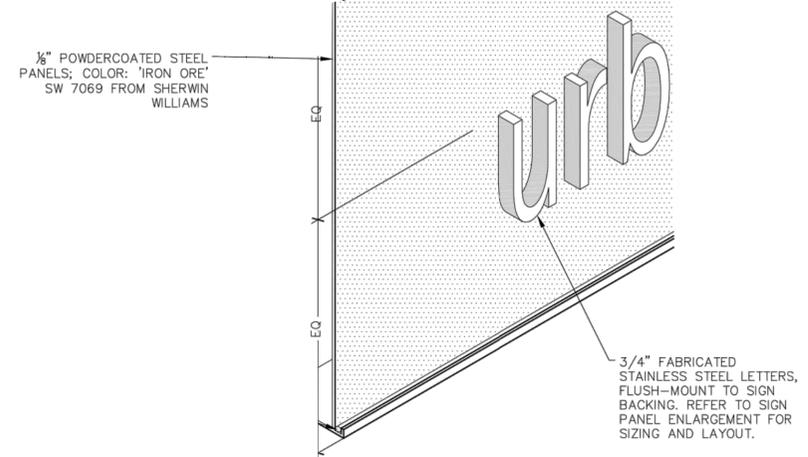
5 RENDERED ELEVATION

NOT TO SCALE



6 ENTRY ARCH SIGN PANEL ENLARGEMENT

NOT TO SCALE



7 ENTRY ARCH METAL CHANNEL ENLARGEMENT

NOT TO SCALE

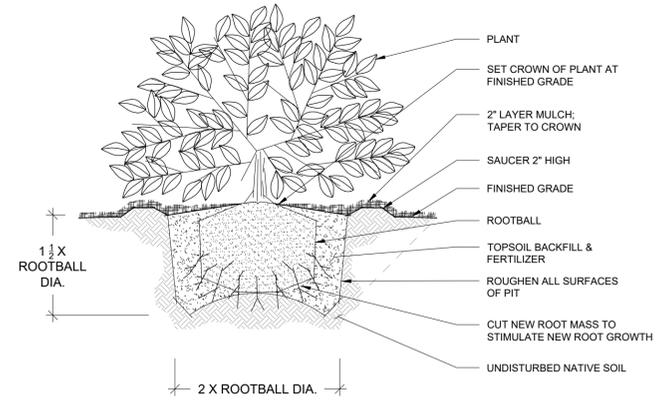
4/20/2020 9:17 AM H:\2019\19187\LANDSCAPE\FINAL SHEETS\19187 URBANE VILLAGE ENTRY ARCH.DWG

NO.	REVISIONS	DATE
<b>CORE DESIGN</b> CIVIL ENGINEERING LANDSCAPE ARCHITECTURE PLANNING SURVEYING		
12100 NE 195th St, Suite 300, Bothell, Washington 98011 425.885.7877		
<b>ARCHWAY LOCATION AND DETAILS</b> <b>URBANE VILLAGE ENTRY ARCHWAY</b> <b>PULTE HOMES OF WASHINGTON</b> 3535 FACTORY BLVD., SUITE 110 BELLEVUE, WASHINGTON 98006		
DATE	APRIL 2020	PROJECT MANAGER
DESIGNED	MANON SU	
DRAWN	MANON SU	
APPROVED	LINDSEY B. SOLORIO, P.L.A.	
	LINDSEY B. SOLORIO, P.L.A.	
SHEET	1	OF 2
PROJECT NUMBER		
<b>19187</b>		



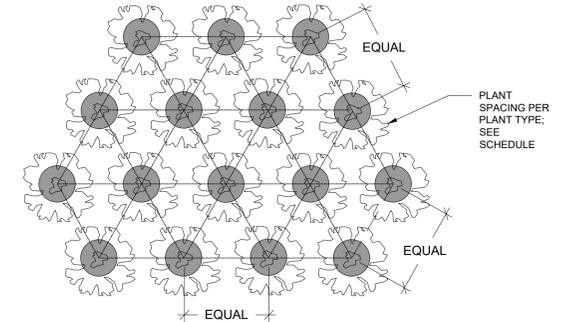
**PLANT SCHEDULE**

TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
	1	MAGNOLIA ACUMINATA 'BUTTERFLIES'	SAUCER MAGNOLIA	2" CAL. MIN.	30' O.C.	FULL/WELL-BRANCHED
SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
	9	HEUCHERA X 'BLACK PEARL' TM	PRIMO BLACK PEARL CORAL BELLS	3 GAL./24" HT. MIN.	2' O.C.	FULL & BUSHY
	5	POLYSTICHUM MUNIUM	WESTERN SWORD FERN	3 GAL./24" HT. MIN.	3' O.C.	FULL & BUSHY
	4	RHODODENDRON X PJM	PJM RHODODENDRON	5 GAL.	36" O.C.	FULL & BUSHY
	6	RHODODENDRON X 'GLACIER'	GLACIER EVERGREEN AZALEA	3 GAL./24" HT. MIN.	3' O.C.	FULL & BUSHY
	2	VIBURNUM DAVIDII	DAVID VIBURNUM	5 GAL./21" MIN. HT.	3' O.C.	FULL & BUSHY



**DETAIL: GROUNDCOVER AND SHRUB PLANTING**

NOT TO SCALE

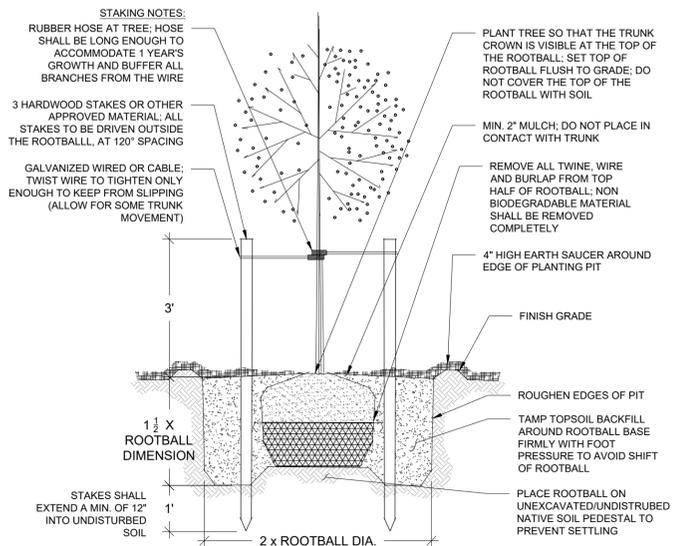


**DETAIL: GROUNDCOVER AND SHRUB SPACING**

NOT TO SCALE

**GENERAL LANDSCAPE NOTES**

- 1.) THE LANDSCAPE CONTRACTOR MUST BE LICENSED AND OR BONDED. CONTRACTOR MUST BE EXPERIENCED IN LANDSCAPE WORK OF THE BEST TRADE PRACTICES AND HAVE THE NECESSARY EQUIPMENT AND PERSONNEL TO PERFORM WORK.
- 2.) THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR FAMILIARIZING HER/HIMSELF WITH THE SITE AND ALL OTHER SITE IMPROVEMENTS PRIOR TO THE START OF LANDSCAPE WORK.
- 3.) THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND USE CAUTION WHILE EXCAVATING IN ORDER TO AVOID DISTURBING ANY EXISTING UTILITIES. THE LANDSCAPE CONTRACTOR WILL PROMPTLY NOTIFY THE GENERAL CONTRACTOR AND OWNER OF ANY CONFLICTS. IN THE EVENT OBSTRUCTIONS ARE ENCOUNTERED DURING PLANTING ACTIVITIES, ALTERNATIVE LOCATIONS MAY BE SELECTED BY THE OWNER'S REPRESENTATIVE.
- 4.) THE LANDSCAPE CONTRACTOR SHALL COORDINATE ALL WORK RELATED TO OTHER TRADES AS REQUIRED.
- 5.) THE LANDSCAPE CONTRACTOR SHALL PROVIDE ALL PLANTS OF THE CORRECT SIZE, SPECIES VARIETY, QUANTITY AND QUALITY AS SPECIFIED ON PLANT SCHEDULE AND SYMBOLS ON LANDSCAPE PLAN. IF UNAVAILABLE, THE LANDSCAPE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE IMMEDIATELY AND PROVIDE THE NAMES AND TELEPHONE NUMBERS OF THREE NURSERY SUPPLIERS OR PLANT BROKERS THAT HAVE BEEN CONTACTED. SUBSTITUTIONS SHALL ONLY BE MADE UPON THE APPROVAL OF THE OWNER'S REPRESENTATIVE OR LANDSCAPE ARCHITECT.
- 6.) ALL PLANT MATERIAL SHALL BE INSPECTED AND APPROVED BY THE OWNER'S REPRESENTATIVE PRIOR TO PLANTING. ALL PLANT MATERIAL SHALL CONFORM TO THE LATEST EDITION OF AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION (ANSI Z60.1).
- 7.) THE LANDSCAPE CONTRACTOR SHALL DELIVER, MAINTAIN AND WATER PLANT MATERIAL UNTIL OWNERS FINAL ACCEPTANCE IS RECEIVED.
- 8.) THE LANDSCAPE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT ALL PROPERTY, INCLUDING PAVEMENT, WALKWAYS, CURBS, FENCING, STRUCTURES, ETC., DURING CONSTRUCTION.
- 9.) THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING INFORMED OF ALL EXISTING CODES, LAWS AND ORDINANCES RELATING TO THE WORK REQUIRED ON SITE, AND SHALL COMPLY ACCORDINGLY.
- 10.) THE LANDSCAPE CONTRACTOR SHALL COORDINATE WITH THE GENERAL CONTRACTOR TO ASSURE PROPER SUBGRADES ARE MET.
- 11.) THE LANDSCAPE CONTRACTOR SHALL SUBMIT THREE REPRESENTATIVE SOIL SAMPLES TO THE OWNER'S REPRESENTATIVE FOR TESTING BY AN APPROVED SOIL TESTING LABORATORY. AMENDMENTS SHALL BE ADDED TO THE SOIL MIX AS RECOMMENDED BY SOIL TESTING LAB.
- 12.) THE LANDSCAPE CONTRACTOR SHALL RAKE PREPARED AREAS AND REMOVE ANY ROCK OR DEBRIS OVER 1". ALL LAWN AREAS SHALL BE ROLLED TO PROVIDE A FIRM, SMOOTH SURFACE FREE OF DIVOTS OR MOUNDS.
- 13.) THE LANDSCAPE CONTRACTOR SHALL SET FINISHED AMENDED SOIL DEPTH BELOW EDGE OF HARDSCAPE ELEMENTS PRIOR TO MULCH AND/OR SOD INSTALLATION. GRADES SHALL BE SET 3" BELOW ADJACENT HARDSCAPE SURFACES FOR PLANTING BEDS AND 1" BELOW FOR LAWN AREAS.
- 14.) THE LANDSCAPE CONTRACTOR SHALL PROVIDE A 2" DEEP LAYER OF MULCH IN ALL PLANTING BEDS.
- 15.) MULCH SHALL BE 100% NATURAL FIR OR HEMLOCK, FINE GROUND, OF UNIFORM COLOR, FREE FROM DYES, WEED SEEDS, SAWDUST & SCRAP LUMBER FIBER, TRASH, INORGANIC MATERIAL OR ANY OTHER COMPOUND DETRIMENTAL TO PLANT GROWTH.
- 16.) FERTILIZER SHALL BE A COMPLETE, BALANCED COMMERCIAL BRAND WITH COMPLETE CHEMICAL ANALYSIS SHOWN ON AN UNOPENED CONTAINER WHEN DELIVERED. FERTILIZER SHALL BE APPLIED AT RATES CONSISTENT WITH THE MANUFACTURER'S RECOMMENDATIONS AND SOIL TESTING LAB'S RECOMMENDATIONS.
- 17.) WORK AREAS TO BE KEPT NEAT AND ORDERLY AND FREE OF DEBRIS AND RUBBISH AT ALL TIMES DURING PROGRESS OF WORK. RAKE BEDS NEATLY TO AN EVEN FINE GRADE AROUND ALL PLANTS. ALL PAVED AREAS ARE TO BE CLEANED BY BROOM AND/OR WASHED AFTER EACH DAY'S WORK OR MORE FREQUENTLY AS REQUIRED. ALL PLANTING AREAS AND ADJACENT PAVED AREAS SHALL BE LEFT IN A NEAT AND CLEAN CONDITION UPON COMPLETION OF JOB.



**DETAIL: PLANTING DECIDUOUS TREE**

NOT TO SCALE

NO.	REVISIONS	DATE



CIVIL ENGINEERING  
LANDSCAPE ARCHITECTURE  
PLANNING  
SURVEYING

**CORE DESIGN**

12100 NE 195th St, Suite 300, Bothell, Washington 98011 425.885.7877

**ARCHWAY LOCATION AND DETAILS**

**URBANE VILLAGE ENTRY ARCHWAY**

**PULTE HOMES OF WASHINGTON**

3535 FACTORY BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

DATE	APRIL 2020
DESIGNED	WANON SU
DRAWN	WANON SU
APPROVED	LINDSEY B. SOLORIO, P.L.A.
	LINDSEY B. SOLORIO, P.L.A.
	PROJECT MANAGER

SHEET	OF
2	2

PROJECT NUMBER  
**19187**

# URBANE VILLAGE II LINEAR PARK

## BOTHELL, WASHINGTON

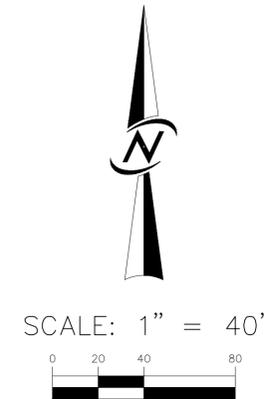


### SHEET INDEX

NO.	DRAWING	SHEET DESCRIPTION
1	L1.01	COVER SHEET
2	L2.01	COMPOSITE LANDSCAPE PLAN
3-4	L2.02 - 2.03	LANDSCAPE PLAN
5	L2.04	LANDSCAPE SCHEDULES AND NOTES
6-7	L2.31 - 2.32	LANDSCAPE DETAILS

### PROJECT NOTES

1. CURRENT SET IS FOR LINEAR PART ONLY. FOR THE REST OF URBANE VILLAGE II, SEE SEPARATE APPROVED SUBMITTAL.
2. CONTRACTOR TO VERIFY ALL TREES ARE INSTALLED 5' MIN FROM ALL UTILITY PIPES, WIRES AND STRUCTURES.
3. SEE SEPARATE SUBMITTAL FOR PEDESTRIAN BRIDGE.
4. ENVIRONMENTAL IMPACT, AND ANY ADDITIONAL REQUIREMENTS FOR SAFETY CONCERN TO BE VERIFIED BY CONTRACTOR BEFORE INSTALLATION.



NO.	DATE	REVISIONS



CIVIL ENGINEERING  
LANDSCAPE ARCHITECTURE  
PLANNING  
SURVEYING

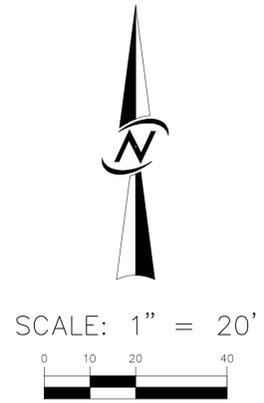
**CORE**  
DESIGN

12100 NE 195th St, Suite 300, Bothell, Washington 98011 425.885.7877

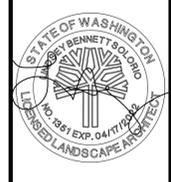
**COVER SHEET**  
**URBANE VILLAGE II LINEAR PARK**  
**PULTE HOMES OF WASHINGTON**  
3535 FACTORIA BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

DATE	JUNE 2020
DESIGNED	WANON SU
DRAWN	WANON SU
APPROVED	LINDSEY B. SOLORIO, P.L.A.
	LINDSEY B. SOLORIO, P.L.A.
	PROJECT MANAGER

SHEET	OF
<b>L1.01</b>	<b>7</b>
PROJECT NUMBER	
<b>19187</b>	



NO.	REVISIONS	DATE



CIVIL ENGINEERING  
LANDSCAPE ARCHITECTURE  
PLANNING  
SURVEYING

**CORE**  
DESIGN

12100 NE 195th St, Suite 300, Bothell, Washington 98011 425.885.7877

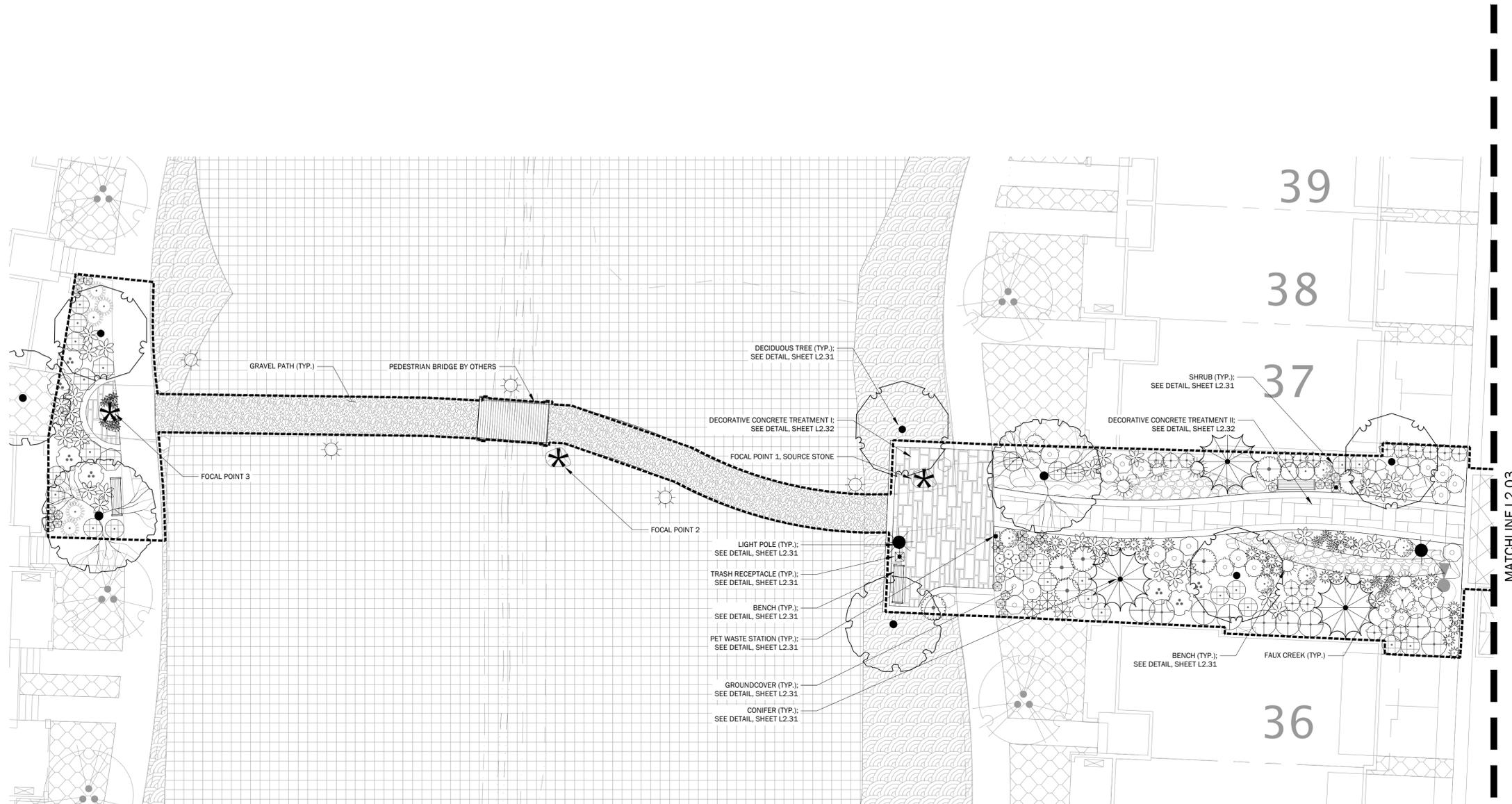
**COMPOSITE LANDSCAPE PLAN**  
**URBANE VILLAGE II LINEAR PARK**

**PULTE HOMES OF WASHINGTON**  
3535 FACTORY BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

DATE: JUNE 2020  
DESIGNED: WANON SU  
DRAWN: WANON SU  
APPROVED: LINDSEY B. SOLORIO, P.L.A.  
LINDSEY B. SOLORIO, P.L.A.  
PROJECT MANAGER

SHEET OF  
**L2.01** **7**

PROJECT NUMBER  
**19187**



MATCHLINE L2.03



SCALE: 1" = 10'



LANDSCAPE PLAN

SCALE: 1" = 10'

DATE	JUNE 2020
DESIGNED	WANON SU
DRAWN	WANON SU
APPROVED	LINDSEY B. SOLORIO, P.L.A.
	LINDSEY B. SOLORIO, P.L.A.
	PROJECT MANAGER
NO.	
REVISIONS	
DATE	
<p>CIVIL ENGINEERING LANDSCAPE ARCHITECTURE PLANNING SURVEYING</p> <p><b>CORE</b> DESIGN</p> <p>12100 NE 195th St, Suite 300, Bothell, Washington 98011 425.885.7877</p>	
<p><b>LANDSCAPE PLAN</b> <b>URBANE VILLAGE II LINEAR PARK</b> <b>PULTE HOMES OF WASHINGTON</b> 3535 FACTORY BLVD., SUITE 110 BELLEVUE, WASHINGTON 98006</p>	
SHEET	OF
<b>L2.02</b>	<b>7</b>
PROJECT NUMBER	
<b>19187</b>	

PROJECT NOTES

- 1. ADDITIONAL SITE LIGHTING TO BE PROVIDED WITH ARCHITECTURAL PLANS TO INCLUDE LIGHTING ON BUILDINGS.
- 2. BENCHES: PACIFIC OUTDOOR PRODUCTS MODEL #SE-5145, SURFACE MOUNTED ON A 5" CONCRETE PAD.
- 3. BIKE RACK: PACIFIC OUT DOOR BICYCLE RACKS (MODEL #SE-5590), MOUNTED ON A 5" CONCRETE PAD.
- 4. ELEMENTS SHOWN WITHIN LINEAR PARK (NODES, DRAINAGE CHANNEL, FOCAL ELEMENTS, ETC.) ARE CONCEPTUAL IN NATURE AND TO BE DEVELOPED WITH FUTURE SUBMITTAL.



SCALE: 1" = 10'



LANDSCAPE PLAN

SCALE: 1" = 10'

NO.	REVISIONS	DATE



CIVIL ENGINEERING  
LANDSCAPE ARCHITECTURE  
PLANNING  
SURVEYING



12100 NE 195th St, Suite 300, Bothell, Washington 98011 425.885.7877

**LANDSCAPE PLAN**  
**URBANE VILLAGE II LINEAR PARK**  
**PULTE HOMES OF WASHINGTON**  
3535 FACTORY BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

DATE	JUNE 2020
DESIGNED	WANION SU
DRAWN	WANION SU
APPROVED	LINDSEY B. SOLORIO, P.L.A.
	LINDSEY B. SOLORIO, P.L.A.
	PROJECT MANAGER

SHEET	OF
<b>12.03</b>	<b>7</b>
PROJECT NUMBER	
<b>19187</b>	

PLANTING SCHEDULE

TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
	1	ACER GIRCINATUM	VINE MAPLE	1.5" CAL. MIN.	AS SHOWN	FULL/WELL BRANCHED
	4	ACER PALMATUM	JAPANESE MAPLE	2" CAL. MIN.	AS SHOWN	FULL/WELL BRANCHED
	3	ACER PALMATUM 'ATROPURPUREUM'	RED JAPANESE MAPLE	2" CAL. MIN.	AS SHOWN	FULL/WELL BRANCHED
<b>TYPE IV TREES - PARKING AREAS</b>	<b>QTY</b>	<b>BOTANICAL NAME</b>	<b>COMMON NAME</b>	<b>SIZE</b>	<b>SPACING</b>	<b>COMMENTS</b>
	5	MAGNOLIA ACUMINATA 'BUTTERFLIES'	SAUCER MAGNOLIA	2" CAL. MIN.	30' O.C.	FULL/WELL-BRANCHED
<b>TYPE I TREES - SIGHT BARRIER</b>	<b>QTY</b>	<b>BOTANICAL NAME</b>	<b>COMMON NAME</b>	<b>SIZE</b>	<b>SPACING</b>	<b>COMMENTS</b>
	8	THUJA PLICATA 'GREEN GIANT'	GREEN GIANT WESTERN ARBORVITAE	6' HT. MIN.	10' O.C.	FULL/WELL-BRANCHED
<b>SHRUBS</b>	<b>QTY</b>	<b>BOTANICAL NAME</b>	<b>COMMON NAME</b>	<b>SIZE</b>	<b>SPACING</b>	<b>COMMENTS</b>
	2	BUXUS MICROPHYLLA 'GOLDEN TRIUMPH'	GOLDEN TRIUMPH BOXWOOD	2 GAL.	3' O.C.	FULL & BUSHY
	12	CAMELLIA SASANQUA 'BONANZA'	BONANZA CAMELLIA	3 GAL./24" HT. MIN.	4' O.C.	FULL & BUSHY
	28	DAPHNE ODORA 'AUREOMARGINATA'	WINTER DAPHNE	5 GAL.	3' O.C.	FULL & BUSHY
	13	HEUCHERA X 'BLACK PEARL' TM	PRIMO BLACK PEARL CORAL BELLS	3 GAL./24" HT. MIN.	2' O.C.	FULL & BUSHY
	12	JUNIPERUS SQUAMATA 'BLUE STAR'	BLUE STAR JUNIPER	2 GAL.	3' O.C.	FULL & BUSHY
	17	NANDINA DOMESTICA 'GULF STREAM' TM	GULF STREAM HEAVENLY BAMBOO	3 GAL./24" HT. MIN.	3' O.C.	FULL & BUSHY
	76	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	3 GAL./24" HT. MIN.	3' O.C.	FULL & BUSHY
	27	PRUNUS LAUROCERASUS 'OTTO LUYKEN'	LUYKENS LAUREL	2 GAL.	24" O.C.	FULL & BUSHY
	15	RHODODENDRON X PJM	PJM RHODODENDRON	5 GAL.	36" O.C.	FULL & BUSHY
	21	RHODODENDRON X 'GLACIER'	GLACIER EVERGREEN AZALEA	3 GAL./24" HT. MIN.	3' O.C.	FULL & BUSHY
	4	RHODODENDRON X 'RAMAPO'	RAMAPO RHODODENDRON	3 GAL./24" HT. MIN.	3' O.C.	FULL & BUSHY
	7	RIBES SANGUINEUM 'KING EDWARD VII'	RED FLOWERING CURRANT	5 GAL.	4' O.C.	FULL & BUSHY
	5	SYMPHORICARPOS ALBUS	COMMON WHITE SNOWBERRY	5 GAL.	4" O.C.	FULL & BUSHY
	29	VACCINIUM OVATUM	EVERGREEN HUCKLEBERRY	5 GAL.	4' O.C.	FULL & BUSHY
	33	VIBURNUM DAVIDII	DAVID VIBURNUM	5 GAL./21" MIN. HT.	3' O.C.	FULL & BUSHY
<b>GRASSES</b>	<b>QTY</b>	<b>BOTANICAL NAME</b>	<b>COMMON NAME</b>	<b>SIZE</b>	<b>SPACING</b>	<b>COMMENTS</b>
	45	ACORUS GRAMINEUS 'OGON'	GOLDEN VARIEGATED SWEETFLAG	2 GAL./15" HT. MIN.	2' O.C.	
	6	CAREX ALBULA	FROSTY CURLS SEDGE	2 GAL./15" HT. MIN.	2' O.C.	
	32	FESTUCA GLAUCA 'ELIJAH BLUE'	BLUE FESCUE	3 GAL./24" HT. MIN.	2' O.C.	
	8	OPHIPOGON JAPONICUS 'NIGRA'	BLACK MONDO GRASS	3 GAL./24" HT. MIN.	18" O.C.	
	6	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	LITTLE BUNNY FOUNTAIN GRASS	3 GAL./24" HT. MIN.	2' O.C.	
<b>TYPE IV BUFFER - PARKING AREAS</b>	<b>QTY</b>	<b>BOTANICAL NAME</b>	<b>COMMON NAME</b>	<b>SIZE</b>	<b>SPACING</b>	<b>COMMENTS</b>
	19	CORNUS SERICEA 'KELSEY'	KELSEYI DOGWOOD	5 GAL./21" MIN. HT.	3' O.C.	FULL & BUSHY
<b>GROUND COVERS</b>	<b>QTY</b>	<b>BOTANICAL NAME</b>	<b>COMMON NAME</b>	<b>SIZE</b>	<b>SPACING</b>	<b>COMMENTS</b>
	111	RUBUS CALYCINOIDES 'EMERALD CARPET'	EMERALD CARPET CREEPING RASPBERRY	2 GAL.	18" O.C.	TRIANGULATE SPACING

AMENITY SCHEDULE

LEGEND	QUANTITY	ITEM	DESCRIPTION
	375 SF	DECORATIVE CONCRETE TREATMENT I	CONCRETE SLAB WITH 3' X 10' SCORING, W/12" SCORED BAND. COLOR: MESQUITE (#667) BY DAVIS COLORS. TO BE SEALED WITH 'COLOR SEAL II' SEALANT BY DAVIS COLORS, IN CORRESPONDING COLOR
	915 SF	DECORATIVE CONCRETE TREATMENT II	CONCRETE SLAB WITH 4' X 4' SQUARE GRID, W/12" SCORED BAND (HAND TOOLED 3/8" X 3/4"). COLOR: BEIGE (GRID PORTION ONLY), OR APPROVED EQUAL. FINISH: LIGHT BROOM. SEE DETAIL ON SHEET L2.32.
	638 SF	DECORATIVE CONCRETE TREATMENT III	CONCRETE SLAB WITH 4' X 4' SQUARE GRID, W/12" SCORED BAND (HAND TOOLED 3/8" X 3/4"). COLOR: BEIGE (GRID PORTION ONLY), OR APPROVED EQUAL. FINISH: LIGHT BROOM. SEE DETAIL ON SHEET L2.32.
	4	LIGHT POLE	LANDSCAPE FORMS, ALCOTT AREA LIGHT, 16' TALL, TYPE 5. SEE PRODUCT DRAWING ON SHEET L2.31
	13	PATH LIGHT	HAWTHORNE PATH LIGHT, CONTRACTOR TO PROVIDE ANCHORING HARDWARE. SEE PRODUCT DRAWING ON SHEET L2.31
	4	BENCH	PACIFIC OUTDOOR PRODUCTS, BENCH (MODEL SE-5115), BLACK POWER COATED
	4	TRASH RECEPTACLE	PACIFIC OUTDOOR PRODUCTS, TRASH RECEPTACLE (MODEL SE-5630), BLACK POWER COATED
	2	PET WASTE STATION	PET WASTE STATION, BY PET WASTE ELIMINATOR, BLACK POWDER COATED, OR SIMILAR PRODUCT APPROVED BY OWNER. SEE DETAIL ON SHEET L2.31

GENERAL LANDSCAPE NOTES

- THE LANDSCAPE CONTRACTOR MUST BE LICENSED AND OR BONDED. CONTRACTOR MUST BE EXPERIENCED IN LANDSCAPE WORK OF THE BEST TRADE PRACTICES AND HAVE THE NECESSARY EQUIPMENT AND PERSONNEL TO PERFORM WORK.
- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR FAMILIARIZING HER/HIMSELF WITH THE SITE AND ALL OTHER SITE IMPROVEMENTS PRIOR TO THE START OF LANDSCAPE WORK.
- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND USE CAUTION WHILE EXCAVATING IN ORDER TO AVOID DISTURBING ANY EXISTING UTILITIES. THE LANDSCAPE CONTRACTOR WILL PROMPTLY NOTIFY THE GENERAL CONTRACTOR AND OWNER OF ANY CONFLICTS. IN THE EVENT OBSTRUCTIONS ARE ENCOUNTERED DURING PLANTING ACTIVITIES, ALTERNATIVE LOCATIONS MAY BE SELECTED BY THE OWNER'S REPRESENTATIVE.
- THE LANDSCAPE CONTRACTOR SHALL COORDINATE ALL WORK RELATED TO OTHER TRADES AS REQUIRED.
- THE LANDSCAPE CONTRACTOR SHALL PROVIDE ALL PLANTS OF THE CORRECT SIZE, SPECIES VARIETY, QUANTITY AND QUALITY AS SPECIFIED ON PLANT SCHEDULE AND SYMBOLS ON LANDSCAPE PLAN. IF UNAVAILABLE, THE LANDSCAPE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE IMMEDIATELY AND PROVIDE THE NAMES AND TELEPHONE NUMBERS OF THREE NURSERY SUPPLIERS OR PLANT BROKERS THAT HAVE BEEN CONTACTED. SUBSTITUTIONS SHALL ONLY BE MADE UPON THE APPROVAL OF THE OWNER'S REPRESENTATIVE OR LANDSCAPE ARCHITECT.
- ALL PLANT MATERIAL SHALL BE INSPECTED AND APPROVED BY THE OWNER'S REPRESENTATIVE PRIOR TO PLANTING. ALL PLANT MATERIAL SHALL CONFORM TO THE LATEST EDITION OF AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION (ANSI Z60.1).
- THE LANDSCAPE CONTRACTOR SHALL DELIVER, MAINTAIN AND WATER PLANT MATERIAL UNTIL OWNERS FINAL ACCEPTANCE IS RECEIVED.
- THE LANDSCAPE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT ALL PROPERTY, INCLUDING PAVEMENT, WALKWAYS, CURBS, FENCING, STRUCTURES, ETC. DURING CONSTRUCTION.
- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING INFORMED OF ALL EXISTING CODES, LAWS AND ORDINANCES RELATING TO THE WORK REQUIRED ON SITE, AND SHALL COMPLY ACCORDINGLY.
- THE LANDSCAPE CONTRACTOR SHALL COORDINATE WITH THE GENERAL CONTRACTOR TO ASSURE PROPER SUBGRADES ARE MET, WHICH SHALL BE 7" IN LAWN AREAS AND 9" ALL PLANTING BEDS.
- THE LANDSCAPE CONTRACTOR SHALL SUBMIT THREE REPRESENTATIVE SOIL SAMPLES TO THE OWNER'S REPRESENTATIVE FOR TESTING BY AN APPROVED SOIL TESTING LABORATORY. AMENDMENTS SHALL BE ADDED TO THE SOIL MIX AS RECOMMENDED BY SOIL TESTING LAB.
- THE LANDSCAPE CONTRACTOR SHALL RAKE PREPARED AREAS AND REMOVE ANY ROCK OR DEBRIS OVER 1". ALL LAWN AREAS SHALL BE ROLLED TO PROVIDE A FIRM, SMOOTH SURFACE FREE OF DIVOTS OR MOUNDS.
- THE LANDSCAPE CONTRACTOR SHALL SET FINISHED AMENDED SOIL DEPTH BELOW EDGE OF HARDSCAPE ELEMENTS PRIOR TO MULCH AND/OR SOD INSTALLATION. GRADES SHALL BE SET 3" BELOW ADJACENT HARDSCAPE SURFACES FOR PLANTING BEDS AND 1" BELOW FOR LAWN AREAS.
- THE LANDSCAPE CONTRACTOR SHALL PROVIDE A 2" DEEP LAYER OF MULCH IN ALL PLANTING BEDS.
- MULCH SHALL BE 100% NATURAL FIR OR HEMLOCK, FINE GROUND, OF UNIFORM COLOR, FREE FROM DYES, WEED SEEDS, SAWDUST & SCRAP LUMBER FIBER, TRASH, INORGANIC MATERIAL OR ANY OTHER COMPOUND DETRIMENTAL TO PLANT GROWTH.
- FERTILIZER SHALL BE A COMPLETE, BALANCED COMMERCIAL BRAND WITH COMPLETE CHEMICAL ANALYSIS SHOWN ON AN UNOPENED CONTAINER WHEN DELIVERED. FERTILIZER SHALL BE APPLIED AT RATES CONSISTENT WITH THE MANUFACTURER'S RECOMMENDATIONS AND SOIL TESTING LAB'S RECOMMENDATIONS.
- WORK AREAS TO BE KEPT NEAT AND ORDERLY AND FREE OF DEBRIS AND RUBBISH AT ALL TIMES DURING PROGRESS OF WORK. RAKE BEDS NEATLY TO AN EVEN FINE GRADE AROUND ALL PLANTS. ALL PAVED AREAS ARE TO BE CLEANED BY BROOM AND/OR WASHED AFTER EACH DAY'S WORK OR MORE FREQUENTLY AS REQUIRED. ALL PLANTING AREAS AND ADJACENT PAVED AREAS SHALL BE LEFT IN A NEAT AND CLEAN CONDITION UPON COMPLETION OF JOB.
- IF A DISCREPANCY EXISTS BETWEEN THE PLANT QUANTITIES ON THE PLANT SCHEDULE AND THOSE SHOWN ON THE PLAN THE QUANTITIES ON THE PLAN SHALL GOVERN.
- PROPOSALS FOR PLANT SUBSTITUTIONS, LOCATION ADJUSTMENTS, SOIL AMENDMENTS OR ANY VARIATIONS FROM THE APPROVED PLANS SHALL REQUIRE PRIOR APPROVAL BY THE RESPONSIBLE OFFICIAL.
- DRAINAGE: CONTRACTOR SHALL NOTIFY THE OWNER OF ANY LOW POINTS OR FORESEEN POOR DRAINING AREAS EXISTING ON-SITE AND PROVIDE CORRECTIVE DRAINAGE PLANS PRIOR TO COMMENCING LANDSCAPE WORK. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING POSITIVE DRAINAGE IN ALL FINISHED LANDSCAPE AREAS THAT ARE PART OF THIS SCOPE OF WORK. ALL POSITIVE DRAINAGE FROM LANDSCAPE AREAS SHALL BE DISCHARGED APPROPRIATELY AND SHALL NOT CREATE DRAINAGE PROBLEMS OFF-SITE OR IN OTHER AREAS OF THE PROJECT. FINISHED LANDSCAPE AREAS WITH PONDING WATER OR OTHER POOR DRAINAGE CONDITIONS SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE.
- LAWNS SHALL BE HYDROSEEDDED WITH "SIGNATURE" AS SUPPLIED BY JB INSTANT LAWN OR APPROVED EQUAL. THE SEEDED AREA MUST BE PROTECTED BY A BARRIER WITH SIGNS THAT READ "KEEP OFF NEWLY SEEDED LAWN AREA".
- OWNER SHALL APPROVE PLANT MATERIAL & PLANTING BED LOCATION PRIOR TO INSTALLATION. ALLOW 24 HOURS MINIMUM NOTIFICATION FOR INSPECTION REQUEST. PLANT MATERIAL THAT HAS BEEN APPROVED FOR INSTALLATION SHALL BE PLANTED WITHIN 24 HOURS. INSTALLATION SHALL NOT BE CONDUCTED UNDER ADVERSE WEATHER CONDITIONS WITHOUT PRIOR APPROVAL OF THE RESPONSIBLE OFFICIAL. PLANT MATERIAL THAT CANNOT BE PLANTED WITHIN ONE DAY FOLLOWING ARRIVAL SHALL BE HELED-IN, KEPT MOIST AND PROTECTED AT ALL TIMES FROM EXTREME WEATHER CONDITIONS. PLANTS SHALL BE STORED AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- TREE PITS SHALL BE A MINIMUM OF TWO TIMES (2x) THE DIAMETER OF THE TREE'S ROOT MASS. ADDITIONAL AERATION MAY BE REQUIRED AS DIRECTED BY THE RESPONSIBLE OFFICIAL. ADD WATER TUBES TO THE TREE PLANTINGS IN PAVED AREAS.
- STREET TREES SHALL BE SYMMETRICAL AND UNIFORM IN APPEARANCE, SIZE AND STRUCTURE.
- STREET TREE AND SHRUB INSTALLATIONS SHALL CONFORM TO THE FOLLOWING GENERAL GUIDELINES:
  - TREES SHALL NOT BE PLANTED IN LOCATIONS THAT COULD LEAD TO ROOTS DAMAGING SIDEWALKS OR CURBING, OR IN ANY OTHER LOCATION THAT MAY CAUSE A SAFETY CONCERN.
- PLANT MATERIALS SHALL BE GUARANTEED FOR A PERIOD OF TWO YEARS. PLANT MATERIAL THAT HAS LOST MORE THAN 30 PERCENT OF ITS NORMAL FOLIAGE SHALL BE REPLACED AT CONTRACTOR'S EXPENSE AS DIRECTED BY THE RESPONSIBLE OFFICIAL.

NO.	REVISIONS	DATE

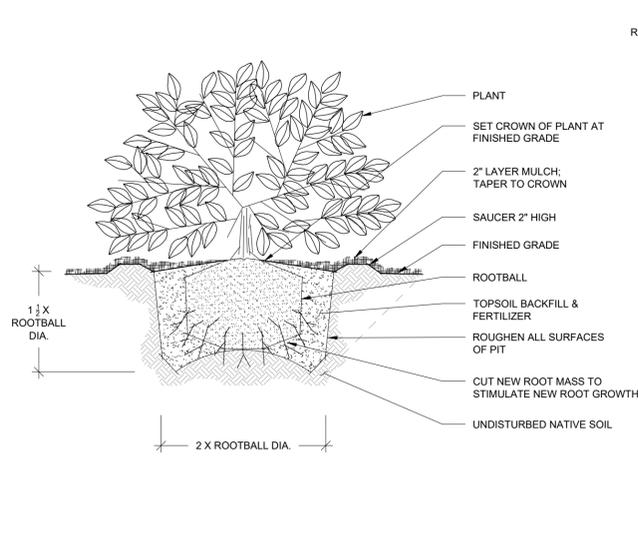


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**CORE DESIGN**  
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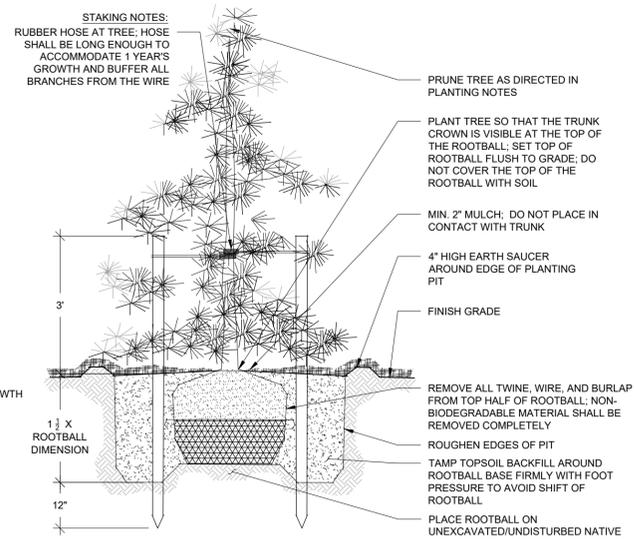
**LANDSCAPE SCHEDULES & NOTES**  
**URBANE VILLAGE II LINEAR PARK**  
**PULTE HOMES OF WASHINGTON**  
3535 FACTORY BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

DATE	JUNE 2020
DESIGNED	WANON SU
DRAWN	WANON SU
APPROVED	LINDSEY B. SOLORIO, P.L.A.
	LINDSEY B. SOLORIO, P.L.A.
	PROJECT MANAGER

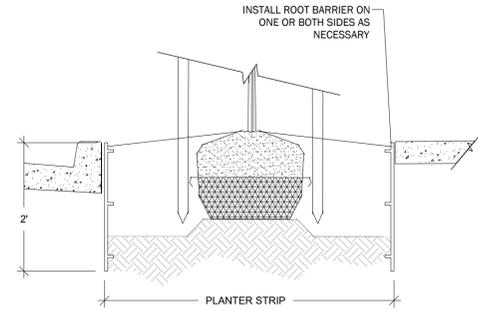
SHEET	OF
<b>L2.04</b>	<b>7</b>
PROJECT NUMBER	
<b>19187</b>	



DETAIL: PLANTING SHRUB  
NOT TO SCALE

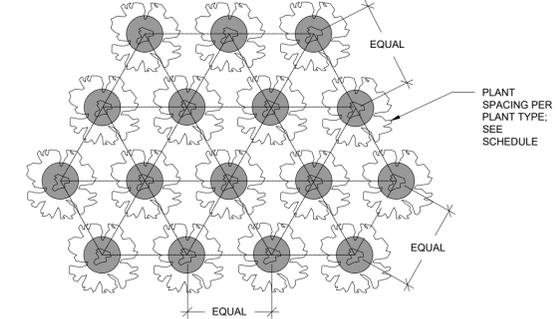


DETAIL: PLANTING CONIFER TREE  
NOT TO SCALE



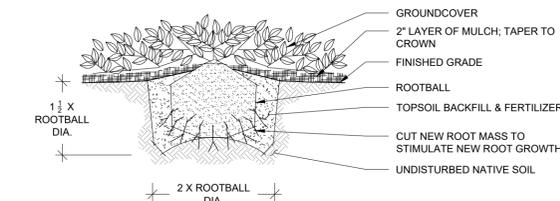
NOTE:  
1. INSTALL PRODUCT PER MANUFACTURER'S SPECIFICATIONS.  
2. INSTALL PRODUCT A MINIMUM OF 3' BEYOND CENTER OF TREE IN EACH DIRECTION PARALLEL TO UTILITY LINE.  
3. ALL TREES LOCATED WITHIN CLOSE PROXIMITY TO UTILITIES TO RECEIVE ROOT BARRIER.

DETAIL: ROOT BARRIER  
NOT TO SCALE

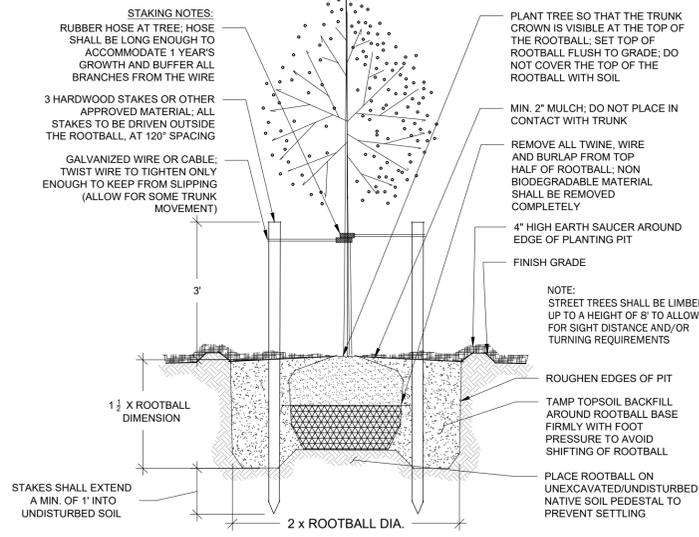


NOTE:  
TO AVOID LANDSCAPE INTRUSION INTO PATHS, THE FOLLOWING PLANTING SPACING OFF-SETS FROM EDGE OF ALL PATHS, DRIVES, ACCESS ROUTES, ETC. SHALL BE MET:  
GROUND COVER: 18"  
SMALL SHRUBS (UNDER 3' TALL): 30"  
MEDIUM SHRUBS (FROM 3' TALL TO 6'): 42"  
LARGE SHRUBS (OVER 6' TALL): 48"

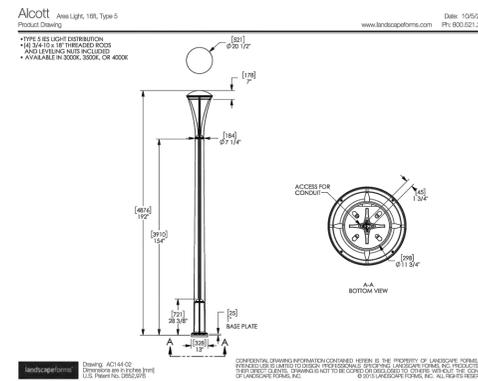
DETAIL: PLANTING GROUNDCOVER SPACING  
NOT TO SCALE



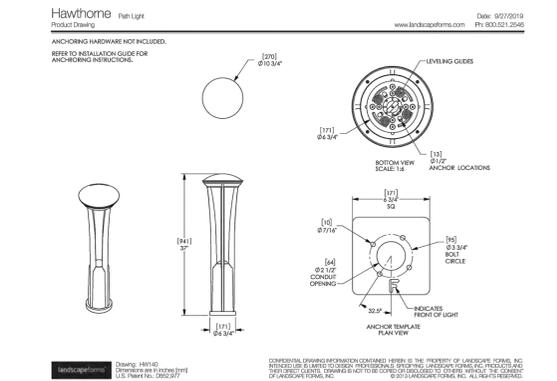
DETAIL: PLANTING GROUNDCOVER  
NOT TO SCALE



DETAIL: PLANTING DECIDUOUS TREE  
NOT TO SCALE

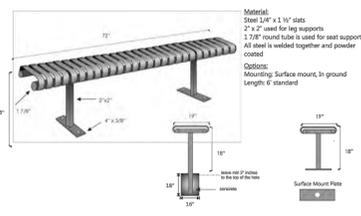


DETAIL: LIGHT POLE  
SOURCE: LANDSCAPE FORMS  
NOT TO SCALE



DETAIL: PATH LIGHT  
SOURCE: LANDSCAPE FORMS  
NOT TO SCALE

SE-5115 Bench Specifications

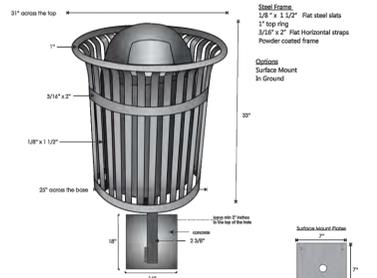


- To Ground**
- 1) Set the bench in the location where it is to be installed, mark the ground where the 2 legs are going to be cemented in the ground and remove the bench and dig the holes 18" deep.
  - 2) Place the bench in the holes and make sure the frame is "plumb and level".
  - 3) Pour the concrete in the holes so it is no more than 2" from the top of each hole. Make sure the top of bench is approximately 18" above the finished grade.
- Surface Mount**
- 1) Set on the concrete where it is to be installed and mark where the mounting holes are to be drilled on the concrete. Remove the bench and drill the holes and both the bench down.

NOTE: Graphics are not to scale. PACIFIC OUTDOOR PRODUCTS, INC. www.pacificoutdoor.com

DETAIL: BENCH  
SOURCE: PACIFIC OUTDOORS  
NOT TO SCALE

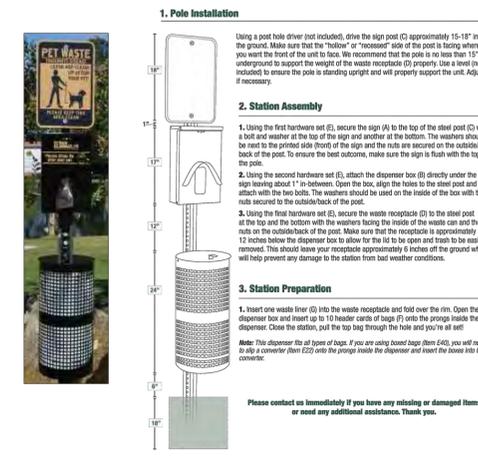
SE-5630 Trash Receptacle Specifications



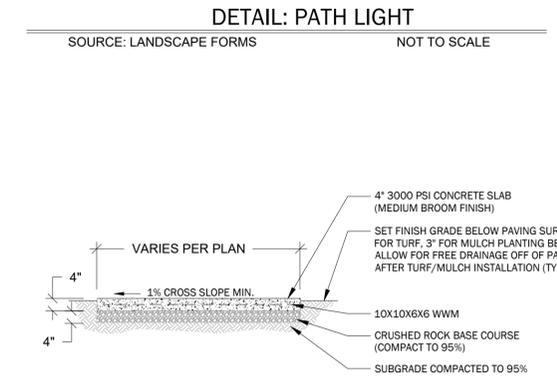
- To Ground Installation:**
- 1) Place the trash receptacle on the ground.
  - 2) Place the base frame where it is to be installed and mark the ground where the 18" x 18" deep ground hole will be dug.
  - 3) Once the hole is dug, place the frame in it making sure it is level and "plumb and level".
  - 4) Using premix concrete, fill all the holes 2" down from the top of the hole.
- Surface Mount Installation:**
- 1) Place trash receptacle on cement surface and mark cement surface through all the holes in the mounting plate.
  - 2) Remove and drill all marked spots with a 5/8" concrete drill bit.
  - 3) Place on the marks/holes and tighten down anchor bolts.

NOTE: Graphics are not to scale. PACIFIC OUTDOOR PRODUCTS, INC. www.pacificoutdoor.com

DETAIL: TRASH RECEPTACLE  
SOURCE: PACIFIC OUTDOORS  
NOT TO SCALE



DETAIL: PET WASTE STATION  
SOURCE: PET WASTE ELIMINATOR  
NOT TO SCALE



- CONCRETE PAVING NOTES:**
1. INSTALL 1/4" x 2" "DUMMY" JOINTS W/ PRE-MOLDED JOINT MATERIAL @ 12' O.C.
  2. TOOL TO PROVIDE 1/4" VV GROOVES @ 4' O.C. BETWEEN DUMMY JOINTS.
  3. COMPACTION OF MATERIALS TO BE VERIFIED USING MODIFIED PROCTOR TEST.

DETAIL: CONCRETE PATH  
NOT TO SCALE

NO.	REVISIONS	DATE

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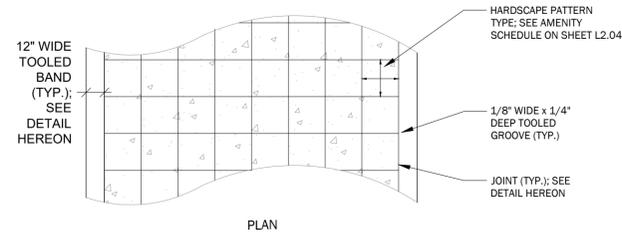
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LANDSCAPE DETAILS  
URBANE VILLAGE II LINEAR PARK  
PULTE HOMES OF WASHINGTON

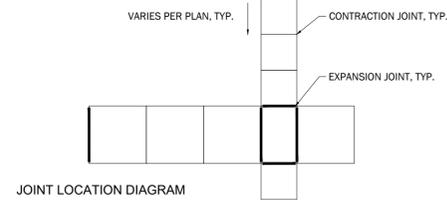
3535 FACTORY BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

DATE	JUNE 2020
DESIGNED	MANON SU
DRAWN	MANON SU
APPROVED	LINDSEY B. SOLORIO, P.L.A.
	LINDSEY B. SOLORIO, P.L.A.
	PROJECT MANAGER

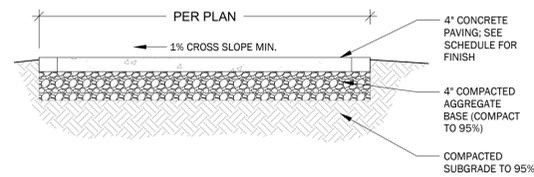
SHEET	OF
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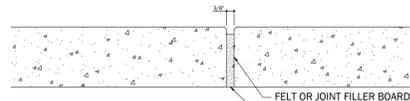
PLAN



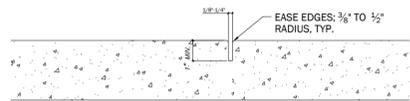
JOINT LOCATION DIAGRAM



SECTION



EXPANSION JOINT



CONTRACTION JOINT

**CONCRETE NOTES:**

1. COLOR TO BE INTEGRATED WITH CONCRETE MIX. TO BE VERIFIED BY CONTRACTOR.

**JOINT NOTES:**

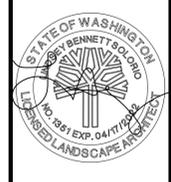
1. UNLESS OTHERWISE NOTED ON PLANS, CONTRACTION JOINTS TO BE SPACED EQUAL TO PATH WIDTH, TYP. (SEE DIAGRAM)

2. EXPANSION JOINT SPACED EVERY THIRD JOINT LOCATION AND AT INTERSECTIONS, TYP. (SEE DIAGRAM)

**PAVEMENT SCORING/JOINTS**

NOT TO SCALE

NO.	REVISIONS	DATE



**CORE DESIGN**

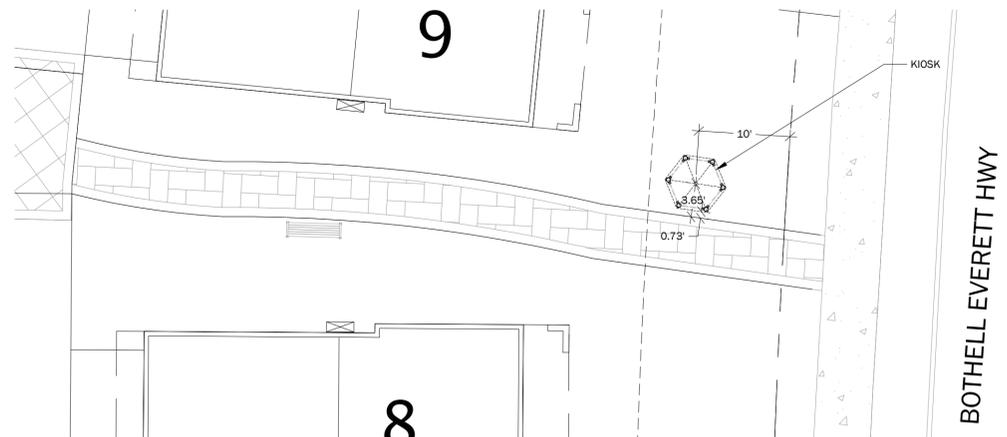
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**LANDSCAPE DETAILS**  
**URBANE VILLAGE II LINEAR PARK**  
**PULTE HOMES OF WASHINGTON**  
3535 FACTORY BLVD., SUITE 110  
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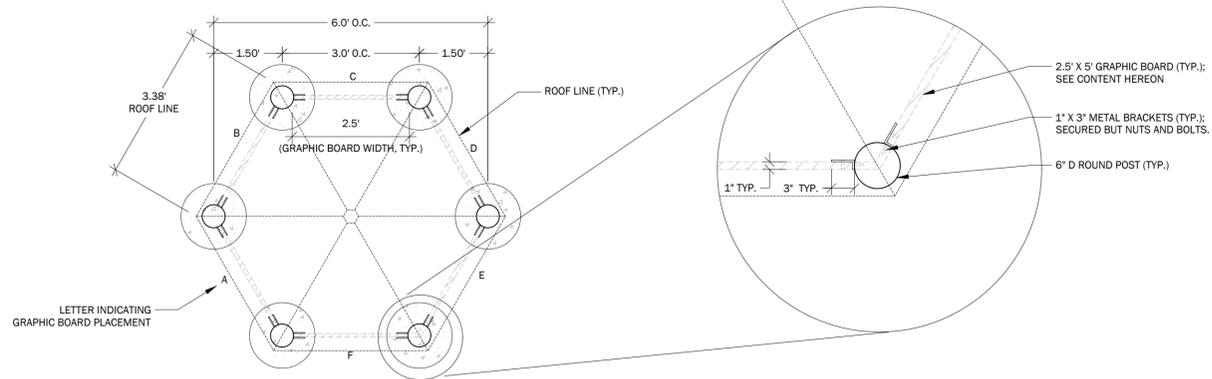
DATE	JUNE 2020
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APPROVED	LINDSEY B. SOLORIO, P.L.A.
	LINDSEY B. SOLORIO, P.L.A.
	PROJECT MANAGER

SHEET	OF
<b>L2.32</b>	<b>7</b>
PROJECT NUMBER	
<b>19187</b>	



KIOSK LOCATION

SCALE: 1" = 10'



KIOSK - PLAN VIEW

SCALE: 1" = 2'

BOARD ATTACHMENT - PLAN VIEW

SCALE: 1" = 1'

### CITY OF BOTHELL EARLY HISTORY

Prior to European settlement, the area of the Squak Slough, now known as the Sammamish River valley, was inhabited by a Native American group loosely translated as the "Willow People" now known as the Sammamish Tribe. The Sammamish lived in the area of present-day Kenmore and Bothell, and migrated seasonally within the region, partially due to the changing levels of the Sammamish river. Following the Point Elliott Treaty of 1855 and Puget Sound War, the Sammamish and other groups were forcibly relocated to reservations and non-reservation lands. Some of the remaining members of the group joined the Snoqualmie, Sishachish, and Skykomish tribes. Some descendants of these groups also later became part of the Tulalip Tribe. To find out more information on the early Native American peoples, visit the Museum of History and Industry located in Seattle. ([www.mohai.com/](http://www.mohai.com/))

Native Americans from Last Sammamish Farms  
Photo courtesy of UW Special Collections

In 1870, Columbus S. Greenleaf and George R. Wilson filed land claims in the area formerly inhabited by the Sammamish near present-day Bothell and built homes. Eight families followed over the next six years. In 1876, Canadian George Brackett bought land and began commercial logging out of a camp located on the north bank of the Sammamish River in what is now the heart of downtown Bothell. The river was the main mode of transport for people and railroad lines were being laid in the late 1880s which opened up routes for more settlers and supplies. Soon a store, school, and sawmills started to define the "downtown Bothell" area.

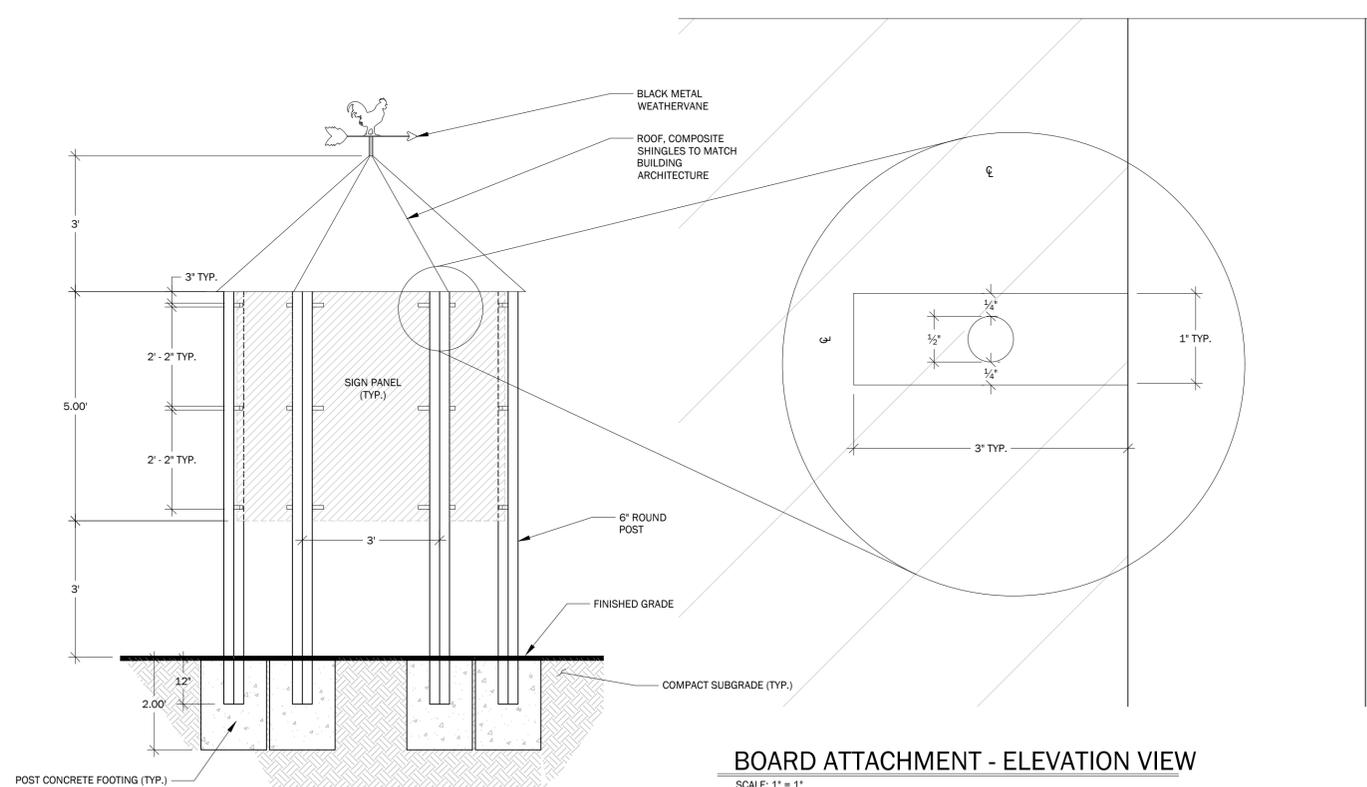
Historical photos and maps showing early settlement, logging, and the founding of the town.

### BOTHELL DOWNTOWN MAIN ST. HISTORICAL WALKING TOUR

Historical photos and maps showing the downtown area, including the Erickson Motor Co. building and other landmarks.

### BOTHELL POPULATION GROWTH MAP

Map showing population growth from 1980 to 2005, with color-coded areas representing different decades.



KIOSK - ELEVATION VIEW

SCALE: 1" = 2'

BOARD ATTACHMENT - ELEVATION VIEW

SCALE: 1" = 1'

GRAPHIC BOARDS (2.5' x 5' TYP.)

SCALE: 1" = 5'

DATE	DESIGNED	DRAWN	APPROVED	PROJECT MANAGER
DECEMBER 2019	WANION SU	WANION SU	LINDSEY B. SOLORIO, P.L.A.	JAMES A. OLSEN, P.E.
SHEET	OF			
1	2			
PROJECT NUMBER 19187				

CIVIL ENGINEERING  
LANDSCAPE ARCHITECTURE  
PLANNING  
SURVEYING

**CORE DESIGN**

12100 NE 195th St, Suite 300 Bothell, Washington 98011 425.885.7877

INFORMATION KIOSK DESIGN  
URBANE VILLAGE II LINEAR PARK  
PULTE HOMES OF WASHINGTON

3535 FACTORY BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

DATE: DECEMBER 2019  
DESIGNED: WANION SU  
DRAWN: WANION SU  
APPROVED: LINDSEY B. SOLORIO, P.L.A.  
PROJECT MANAGER: JAMES A. OLSEN, P.E.

### BOTHELL FOUNDING FAMILIES COUNTRY VILLAGE HOMESTEADS

August Bartelson was born in Denmark in 1855 and emigrated to Washington in 1883 with his wife Anna. They worked in Seattle in a meat and dairy business for a year and then moved onto their homesteaded land in 1884 where they had four children. August also started one of the first schools in the area which was located on his property. In 1897 Bartelson and his neighbor to the south, Joseph Mohr, petitioned the governing school district around Bothell to incorporate the school into their district. They were successful, and the North Creek School was built in the Canyon Park area and became part of the Bothell School District where it functioned until 1920. August Bartelson served on the school board and lived in his homestead house until he died in 1925.



North Creek School  
Photo courtesy of Bothell Images of America by Margaret Turcott



August Bartelson homestead - 1885  
Photo courtesy of Susan Sough 1870-1910  
by Stokney & McDonald



Bartelson Family Standing - August, Johanna, Rosa, Walter, Graham, Sorenson - Mrs. Sorenson with Charles and Yvonnea Bartelson with two young sons of Rosa Sough 1870-1910  
by Stokney & McDonald



Lift Logging Camp  
Photo courtesy of Bothell Historical Museum



Agricultural equipment  
Photo courtesy of Bothell Images of America by Margaret Turcott

The Erickson and Mohr families were very influential in the early development of Bothell. Erickson ran the first mercantile store that featured the first post office. During the economic depression started by the Panic of 1893 Erickson set up a flume in 1895 that ran for approximately seven miles transporting cedar shingle bolts from Mill Creek down to Bothell to be made into shingles at the mill. Mohr was one of the owners of the Cooperative Shingle Mill that started in 1893. Erickson regularly made deliveries to Bothell's families by horse and carriage for his mercantile store, and in 1905 Mohr bought a hardware store that later expanded into a furniture store. In 1904, Erickson joined the Washington state legislature and in 1913 he helped to complete the first highway from Seattle through Bothell to Everett. Mohr served on the local school board for several years following the steps of his neighbor, August Bartelson. Throughout the years, the Mohr's and Erickson's had several houses and properties in and around the Bothell area.



Jacob Mohr house 1888  
Photo courtesy of Bothell Images of America by Margaret Turcott



Erickson's home 1888  
Photo courtesy of Bothell Images of America by Margaret Turcott



Mohr Family  
at Row, Jacob, Adam, Anna  
and four other children, 1890s. Ancestral and Biographical Society of Bothell  
Photo courtesy of Margaret Turcott



Erickson and Orestes Erickson family in the early 1900s. The Erickson had an address of whom few knew to address.  
Photo courtesy of the Bothell Historical Society



Erickson's home 1911  
Photo courtesy of Susan Sough 1870-1910  
by Stokney & McDonald



The Erickson horse-drawn delivery wagon.  
Photo courtesy of the Bothell Historical Museum



Forest Olin came from the Northwest to Washington in the early 1900s where he purchased part of Mohr's homesteaded land and went into the dairy business. Forest's daughters were school teachers in Seattle and Bothell, and Olin served on the local school board. His brother Floyd also moved to the area and worked as a carpenter. He built a house North of Forest close to the old Everett highway on the Bartelson property.



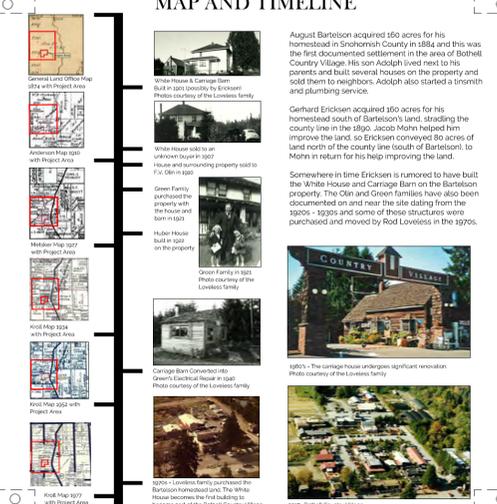
Forest Olin with three children and wife  
Photo courtesy of Susan Sough 1870-1910  
by Stokney & McDonald



Erickson's Mercantile Store  
Photo courtesy of Susan Sough 1870-1910  
by Stokney & McDonald

C

### BOTHELL COUNTRY VILLAGE MAP AND TIMELINE



August Bartelson acquired 150 acres for his homestead in Snohomish County in 1884 and this was the first documented settlement in the area of Bothell Country Village. His son Adolph lived next to his parents and built several houses on the property and sold them to neighbors. Adolph also started a tinmith and plumbing service.

Gerhard Erickson acquired 150 acres for his homestead south of Bartelson's land, straddling the county line in the 1890s. Jacob Mohr helped him improve the land, so Erickson conveyed 80 acres of land north of the county line (south of Bartelson's) to Mohr in return for his help improving the land.

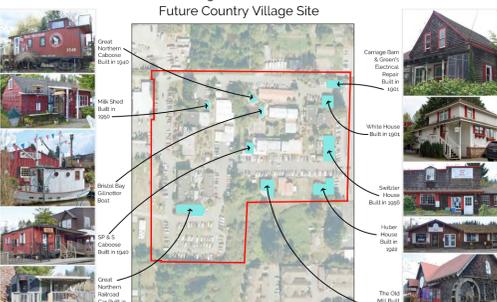
Somewhere in time Erickson is rumored to have built the White House and Carriage Barn on the Bartelson property. The Olin and Green families have also been documented on and near the site dating from the 1920s - 1930s and some of these structures were purchased and moved by Rod Loveliss in the 1970s.

1910 - Loveliss family purchased the Bartelson homestead site. The White House became the first building to become part of the Bothell Country Village.  
Photo courtesy of the Loveliss family

1925 - Bothell Country Village  
Photo courtesy of the Loveliss family

In the late 1970s Rod Loveliss purchased the property to build Bothell Country Village. Loveliss was inspired by the New England town squares, and he and his wife, Barbara, planned to creatively integrate the four existing historic buildings on site with small plazas, water features, gardens and an extensive landscape for an outdoor shopping experience. The Loveliss family also added a collection of antique items, acquired from auctions, into the expansion of the property along with eight new buildings that mimicked the architectural style of the early 1900s. This unique creation was part of the Loveliss family for nearly 40 years.

#### Notable Buildings of Bartelson Farmstead Future Country Village Site

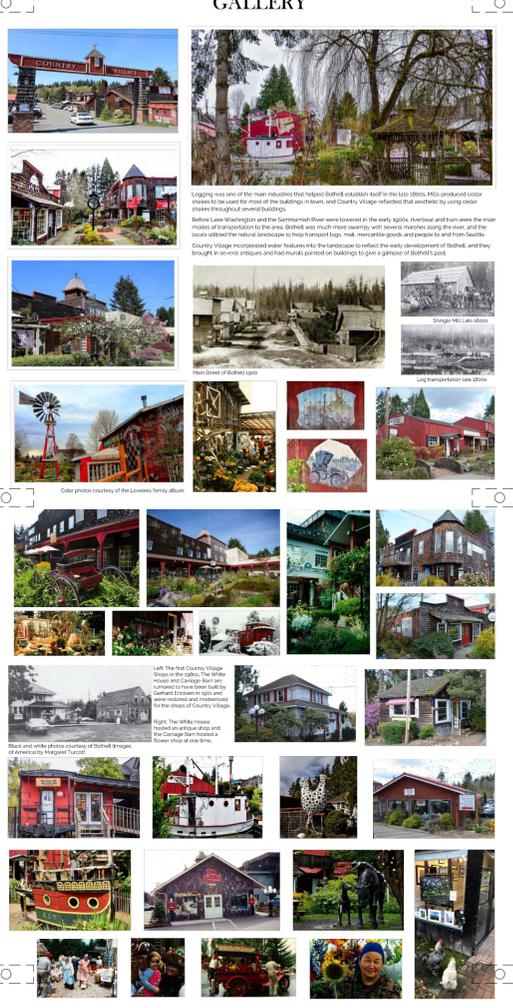


- Great Harvest Cabin  
Built in 1941
- Mill Shed  
Built in 1900
- Black Bay Gateway  
Built in 1921
- Old 45  
Built in 1921
- Old 45  
Built in 1921
- Carriage Barn & Green's Grocery  
Built in 1911
- White House  
Built in 1911
- Saltbox House  
Built in 1920
- Huber House  
Built in 1921
- The Old Mill  
Built in 1910

(Country Village Shopping Center in 2009)

D

### BOTHELL COUNTRY VILLAGE GALLERY



Logging was one of the main industries that helped Bothell establish itself in the late 1800s. Mills produced cedar shingles for use in most of the buildings in town, and Country Village reflected that ancestry by using cedar shingles throughout on several buildings.

Barthelme John Washington and the Carriage Barn were constructed in the early 1900s, and were the main modes of transportation to the area. Bothell was much more seamy with several markets along the river, and the boats helped the house construction to help transport logs, mill, iron, water goods, and people in and from Bothell Country Village incorporated water features into the landscape to reflect the early development of Bothell, and they brought in several antiques and had murals painted on buildings to give a glimpse of Bothell's past.

Left: The Bothell Country Village Shop in the 1980s. The White House and Carriage Barn were rumored to have been built by Gerhard Erickson and were purchased and moved to the site of Country Village.

Right: The White House housed an antique shop and the Carriage Barn housed a flower shop at one time.

Color photo courtesy of the Loveliss family

E

### URBANE VILLAGE COMMUNITY BOARD

F

1" MIN. BETWEEN GRAPHIC CONTENT AND THE EDGE OF THE BOARD (TYP.)

GRAPHIC BOARDS  
SCALE: 1" = 5'

DATE	DESIGNED	DRAWN	APPROVED	PROJECT MANAGER
DECEMBER 2019	WANION SU	WANION SU	LINDSEY B. SOLORIO, P.L.L.C.	JAMES A. OLSEN, P.E.
SHEET	OF	PROJECT NUMBER		
2	2	19187		

CIVIL ENGINEERING  
LANDSCAPE ARCHITECTURE  
PLANNING  
SURVEYING  
DESIGN

**CORE DESIGN**

12100 NE 195th St, Suite 300 Bothell, Washington 98011 425.985.7877

INFORMATION KIOSK DESIGN  
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3535 FACTORIA BLVD., SUITE 110  
BELLEVUE, WASHINGTON 98006

5/12/2020 1:18 PM J:\2019\151877\LANDSCAPE\WOSKA\19187 L - KIOSK DESIGN 3.13.20.DWG

# BOTHELL COUNTY VILLAGE GALLERY



Logging was one of the main industries that helped Bothell establish itself in the late 1800s. Mills produced cedar shakes to be used for most of the buildings in town, and Country Village reflected that aesthetic by using cedar shakes throughout several buildings.

Before Lake Washington and the Sammamish River were lowered in the early 1900s, riverboat and train were the main modes of transportation to the area. Bothell was much more swampy with several marshes along the river, and the locals utilized the natural landscape to help transport logs, mail, mercantile goods and people to and from Seattle.

Country Village incorporated water features into the landscape to reflect the early development of Bothell, and they brought in several antiques and had murals painted on buildings to give a glimpse of Bothell's past.



Main Street of Bothell 1900



Shingle Mill Late 1800s



Log transportation late 1800s



Color photos courtesy of the Loveless family album



Black and white photos courtesy of Bothell (Images of America) by Margaret Turcott

Left: The first Country Village Shops in the 1980s. The White House and Carriage Barn are rumored to have been built by Gerhard Ericksen in 1901 and were restored and modernized for the shops of Country Village.

Right: The White House hosted an antique shop and the Carriage Barn hosted a flower shop at one time.



# BOTHELL FOUNDING FAMILIES

## COUNTRY VILLAGE HOMESTEADS

August Bartelson was born in Denmark in 1855 and emigrated to Washington in 1883 with his wife Anna. They worked in Seattle in a meat and dairy business for a year and then moved onto their homestead land in 1884 where they had four children. August also started one of the first schools in the area which was located on his property. In 1897 Bartelson and his neighbor to the south, Joseph Mohn, petitioned the growing school district around Bothell to incorporate the school into their district. They were successful, and the North Creek School was built in the Canyon Park area and became part of the Bothell School District where it functioned until 1920. August Bartelson served on the school board and lived in his homestead house until he died in 1921.



North Creek School  
Photo courtesy Bothell (Images of America)  
by Margaret Turcott



August Bartelson Homestead - 1884  
Photo courtesy of Squak Slough 1870-1920  
by Stickney & McDonald



Bartelson Family: Standing - August, Adolph, Rosa, Waite Graham; Seated - Mrs. Graham with Chester and "Grandma Bartelson" with Eva. Photo courtesy of Squak Slough 1870-1920 by Stickney & McDonald

Jacob Mohn and Gerhard Ericksen were both from Norway and came to America in 1881. They landed in New York and worked their way west over the next few years practicing a variety of trades. Eventually Ericksen filed for homestead land close to Bothell in 1886 after he and Mohn initially came to the area by working in George Brackett's logging camp.



Jacob Mohn home 1886  
Photo courtesy of Bothell (Images of America)  
by Margaret Turcott

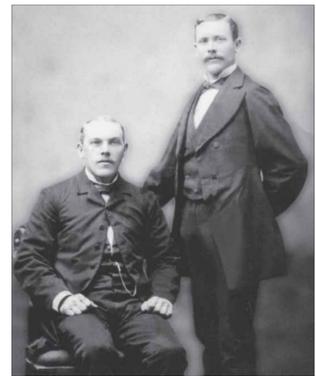


Left: Logging Camp  
Photo courtesy of Bothell  
Historical Museum



Ericksen's Home 1888  
Photo courtesy of Bothell (Images of America)  
by Margaret Turcott

Right: Gerhard Ericksen  
& Jacob Mohn 1884  
Photo courtesy of Bothell (Images of  
America) by Margaret Turcott



The Ericksen and Mohn families were very influential in the early development of Bothell. Ericksen ran the first mercantile store that featured the first post office. During the economic depression started by the Panic of 1893 Ericksen set up a flume in 1895 that ran for approximately seven miles transporting cedar shingle bolts from Mill Creek down to Bothell to be made into shingles at the mill. Mohn was one of the owners of the Cooperative Shingle Mill that started in 1889. Ericksen regularly made deliveries to Bothell's families by horse and carriage for his mercantile store, and in 1909 Mohn bought a hardware store that later expanded into a furniture store. In 1904 Ericksen joined the Washington state legislature and in 1913 he helped to complete the first highway from Seattle through Bothell to Everett. Mohn served on the local school board for several years following in the steps of his neighbor, August Bartelson. Throughout the years, the Mohn's and Ericksen's had several houses and properties in and around the Bothell area.



Mohn Family:  
1st Row: Jacob, Adele, Anna  
2nd Row: Esther, Hanford, Agnes, Arnold and Ragna  
Photo courtesy of Bothell (Images of America)  
by Margaret Turcott



Gerhard and Dorothea Ericksen's family in the early 1900s. The Ericksens had ten children, of whom five lived to adulthood. Photo courtesy of the Bothell Historical Society.



Above: The Cooperative Shingle Mill.  
Photo courtesy of Squak Slough 1870-1920  
by Stickney & McDonald



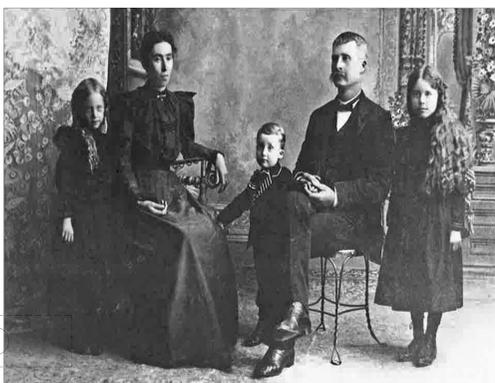
Above: Views of Ericksen's Flume.  
Photo courtesy of Bothell Historical Museum



Ericksen's Home 1913  
Photo courtesy of Squak Slough 1870-1920  
by Stickney & McDonald



The Ericksen horse-drawn delivery wagon.  
Photos courtesy of the Bothell Historical Museum.



Evelyn & Forrest Olin with Erma, Erwin and Merle  
Photo courtesy of Squak Slough 1870-1920 by Stickney & McDonald

Forrest Olin came from the Midwest to Washington in the early 1900s where he purchased part of Mohn's homestead land and went into the dairy business. Forrest's daughters were school teachers in Seattle and Bothell, and Olin served on the local school board. His brother Floyd also moved to the area and worked as a carpenter. He built a house North of Forrest close to the old Everett highway on the Bartelson property.



Ericksen's Mercantile 1892  
Photo courtesy Bothell (Images of America)  
by Margaret Turcott

# CITY OF BOTHELL

## EARLY HISTORY



Women of the Tulalip Tribe carding wool, 1898  
Photo courtesy of UW Special Collections



The Squak 1884-1892  
Photo courtesy of Bothell Historical Museum

Prior to European settlement, the area of the Squak Slough, now known as the Sammamish River valley, was inhabited by a Native American group loosely translated as the "Willow People", now known as the Sammamish Tribe. The Sammamish lived in the area of present-day Kenmore and Bothell and migrated seasonally within the region, partially due to the changing levels of the Sammamish River. Following the Point Elliot Treaty of 1855 and Puget Sound War, the Sammamish and other groups were forcibly relocated to reservations and non-reservation lands. Some of the remaining members of the group joined the Snoqualmie, Snohomish, and Skykomish tribes. Some descendants of these groups also later became part of the Tulalip Tribe. To find out more information on the early Native American peoples, visit the Museum of History and Industry located in Seattle. ([www.mohai.com](http://www.mohai.com))



View of Bothell from Norway Hill 1904  
Photo courtesy of Bothell Historical Museum



Native Americans from Lake Sammamish Friends  
Photo courtesy of UW Special Collections

In 1870, Columbus S. Greenleaf and George R. Wilson filed land claims in the area formerly inhabited by the Sammamish near present-day Bothell and built homes. Eight families followed over the next six years. In 1876, Canadian George Brackett bought land and began commercial logging out of a camp located on the north bank of the Sammamish River in what is now the heart of downtown Bothell. The river was the main mode of transport for people until railroad lines were being laid in the late 1880s which opened up routes for more settlers and supplies. Soon a store, school, and sawmills started to define the "downtown Bothell" area.



City of Bothell Steam Boat early 1900s  
Photos courtesy of Bothell Historical Museum



Early logging railroads  
Photos courtesy of Bothell Historical Museum



Early logging railroads  
Photos courtesy of Bothell Historical Museum



Town of Bothell looking from Norway Hill in 1909  
Photos courtesy of Bothell Historical Museum



Cedar Shingle Mill  
Photos courtesy of Bothell Historical Museum

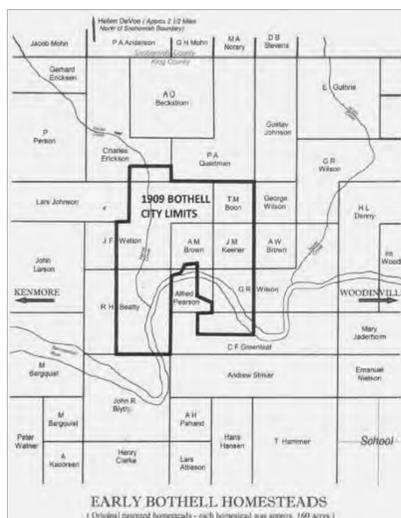


Bothell Railroad Depot  
Photos courtesy of Bothell Historical Museum

In 1885, pioneer George Brackett sold parcels of land totaling 160 acres to David Bothell (1820-1905) in the area that is modern day downtown Bothell. David Bothell then sold his first building lot to Dorothea Ericksen in the 1880s. That lot later became the site of Ericksen's Mercantile, where Gerhard Ericksen, Dorothea's husband started his first store. Ericksen became the first postmaster in 1888, since the post office was located in his store. Before this he had been rowing across the river to Woodinville to pick up the community's mail several times a week. Among his many achievements, Erickson gave Bothell its name in 1888. When asked what the first post office should be called, Ericksen said, "There are so many Bothells in town and that's a good family, so let's call it Bothell." The original street names of downtown Bothell were changed as the town's population grew. Fir, Pine and Cedar streets are now NE 183rd, NE 185th, and NE 186th St. First, Second and Third Streets are now 101st, 102nd and 103rd Ave. NE. Main Street remained the same and still has some original buildings from the first businesses that supported Bothell, including the Ericksen building after it was rebuilt in brick.



Original street names of downtown Bothell.  
Photo courtesy Bothell (Images of America) by Margaret Turcott



The Town of Bothell incorporated in April of 1909 with a population of 599 people.  
Photo courtesy Bothell (Images of America) by Margaret Turcott



The American Hotel built in 1889 burned down in 1926. It housed rooms, a saloon, restaurant, and Bothell's first telephone exchange. Photo courtesy of Bothell Then and Now 1909 - 2009 by Bothell Landmark Preservation Board



David Bothell's boarding house was originally built in 1885. Shown here is the boarding house in 1908 after being rebuilt in due to a fire. Photo courtesy of Bothell Historical Museum



Above: Downtown Main St. 1909  
Photos courtesy of the Bothell Historical Society



Float for Ericksen's Mercantile 1908  
Photo courtesy Bothell (Images of America) by Margaret Turcott



A postcard from 1910 showing the Ericksen store with a view from approximately the same location as the photo above. Photos courtesy of the Bothell Historical Society



Logging photos courtesy of Bothell Historical Museum

# BOTHELL COUNTRY VILLAGE

## MAP AND TIMELINE



General Land Office Map 1874 with Project Area



Anderson Map 1910 with Project Area



Metsker Map 1927 with Project Area



Kroll Map 1934 with Project Area



Kroll Map 1952 with Project Area



Kroll Map 1977 with Project Area



White House & Carriage Barn  
Built in 1901 (possibly by Ericksen)  
Photos courtesy of the Loveless family



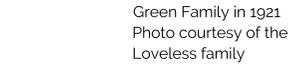
White House sold to an unknown buyer in 1907  
House and surrounding property sold to F.V. Olin in 1910



Green Family purchased the property with the house and barn in 1921



Huber House built in 1922 on the property



Green Family in 1921  
Photo courtesy of the Loveless family



Carriage Barn Converted into Green's Electrical Repair in 1940  
Photo courtesy of the Loveless family

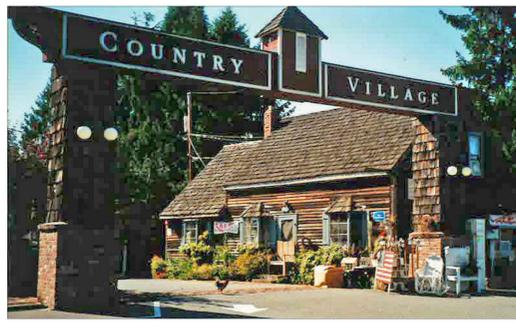


1970s - Loveless family purchased the Bartelson homestead land. The White House becomes the first building to become part of the Bothell Country Village  
Photo courtesy of the Loveless family

August Bartelson acquired 160 acres for his homestead in Snohomish County in 1884 and this was the first documented settlement in the area of Bothell Country Village. His son Adolph lived next to his parents and built several houses on the property and sold them to neighbors. Adolph also started a tinsmith and plumbing service.

Gerhard Ericksen acquired 160 acres for his homestead south of Bartelson's land, straddling the county line in the 1890. Jacob Mohn helped him improve the land, so Ericksen conveyed 80 acres of land north of the county line (south of Bartelson), to Mohn in return for his help improving the land.

Somewhere in time Ericksen is rumored to have built the White House and Carriage Barn on the Bartelson property. The Olin and Green families have also been documented on and near the site dating from the 1920s - 1930s and some of these structures were purchased and moved by Rod Loveless in the 1970s.



1980's - The carriage house undergoes significant renovation.  
Photo courtesy of the Loveless family



2017 - Bothell Country Village  
Photo courtesy of the Loveless family

In the late 1970s Rod Loveless purchased the property to build Bothell Country Village. Loveless was inspired by the New England town squares, and he and his wife, Barbara, planned to creatively integrate the four existing historic buildings on site with small plazas, water features, gardens and an extensive landscape for an outdoor shopping experience. The Loveless family also added a collection of antique items, acquired from auctions, into the expansion of the property along with eight new buildings that mimicked the architectural style of the early 1900s. This unique creation was part of the Loveless family for nearly 40 years.

### Notable Buildings of Bartelson Farmstead Future Country Village Site



Great Northern Caboose  
Built in 1940



Milk Shed  
Built in 1950



Bristol Bay Gillnetter Boat



SP & S Caboose  
Built in 1940



Great Northern Railroad Car  
Built in 1950



(Country Village Shopping Center in 2009)

Carriage Barn & Green's Electrical Repair  
Built in 1901



White House  
Built in 1901



Switzler House  
Built in 1956



Huber House  
Built in 1922



The Old Mill  
Built in 1960



# BOTHELL DOWNTOWN MAIN ST. HISTORICAL WALKING TOUR

Pictures And Information Provided By: Bothell Historical Museum; Bothell, Then And Now 1909-2009 By Bothell Landmark Preservation Board; Squak Slough 1870-1920 By Stickney And McDonald; Bothell Images Of America By Turcott; Little Histories; Bothell Washington By Evans.

**Formerly: Erickson Mercantile building** built 1887; post office added in 1889; rebuilt in brick in 1926; converted to Thriftway in 1957. Currently: the Erickson Building is still owned by Erickson family and a furniture shop occupies original mercantile location.

**Formerly: Severance Bakery** built 1908. Burned down in 1911; built in brick 1928 as Crawford's Drug Store. Currently: Crawford Building.

**Formerly: Mohn Hardware** built 1911. Currently: Main Street Mall.

**Formerly: 1st floor - Mohn Furniture** 2nd floor- Odd Fellow Hall. Currently: Mills Music.

**Formerly: Dexter's Cafe** built 1911; expanded to Keener's Meat Market in 1942. Currently: Logsdon building with shops.

**Formerly: Danish-Norwegian Evangelical Lutheran Midside Church** built 1886. Currently: First Lutheran Church.

**Formerly: Erickson craftsman style house** built 1913. Currently: Safeway.

**Formerly: Erickson Motor Company** built 1946. Currently: Shuck's Auto Supply.

**Formerly: Hannan Building** built 1890; rebuilt in brick 1908. Currently: Bothell Masonic Lodge with facade remodel.

**Formerly: Bothell Hotel**, built 1895; rebuilt in 1908 and torn down in 1954. Currently: rebuilt in brick 1955. Tsuga Custom Framing.

**Formerly: Olympia Bar** built 1893; rebuilt in 1908 as Keystone Club. Currently: Three Lions Pub.

**Formerly: Webster's Blacksmith**, built in 1890's-1900's; burned down in 1908; rebuilt in wood 1911; rebuilt in brick 1927 as George's Cafe; then remodeled to Meredith's 5 & 10 Cent Store. Currently: Alexa's Cafe.

**Formerly: Walters Feed Mill** converted to rail road station. Currently: North Shore Health and Wellness Center.

**Formerly: Cain & Lytte Mill** built 1888; (bought by the Bothell brothers). Currently: Metro Park and Ride lot.

**Formerly: Co-operative Mercantile Company** built 1890; re-built in 1908 in brick. Currently: still standing with facade changes.

**Formerly: Blue Swallow Inn**. Currently: Yakima Fruit Market And Nursery.

**Formerly: Hannon House** First location built 1893. Currently: relocated to the Park at Bothell Landing in 1978 and residence of Bothell Historical Museum.

**Formerly: Hannon House** First location built 1893. Currently: relocated to the Park at Bothell Landing in 1978 and residence of Bothell Historical Museum.

**Formerly: Cain & Lytte Mill** built 1888; (bought by the Bothell brothers). Currently: Metro Park and Ride lot.

**Formerly: Co-operative Mercantile Company** built 1890; re-built in 1908 in brick. Currently: still standing with facade changes.

**Logging and cedar shingle mills** along the river were the economic backbone for the Bothell area during the city's initial development.

**Erickson's seven mile flume**

**Bothell brother's tramway 1885**

# BOTHELL POPULATION GROWTH MAP

**Bothell City Limits Boundary**  
**Erickson Property/ Country Village Site**  
**Bothell City Limits 1909**  
**Bothell City Limits 1938**  
**Bothell City Limits 1950s-1960s**  
**Bothell City Limits 1970s-1980s**  
**Bothell City Limits 1990s**  
**Bothell City Limits 2000s**  
**King/Snohomish County Line**

**Bartelson's homestead land**, converted to Bothell Country Village in 1980s. Currently: Urbane Village development built 2019-2020.

**Mohn's homestead** Built 1888.

**Erickson's homestead** Built 1888.

**Bothell downtown Main Street, Early 1900's**

**Formerly: String Town** (built in 1890's) String Town was a row of houses built along the road to Woodinville near the bridge. A few of the early families linked to this area are the Davenport, Ervays, Fish family and Dr. Chase's house and hospital. Currently: University of Washington, Bothell built in 1990's. Dr. Chase's home has been preserved on campus.

**Formerly: Davenport House** Built 1889.

**Formerly: Dr. Chase's House** Built 1889.

**Formerly: Dr. Chase's hospital** Built 1889.

**Red brick road** stretching 2 miles at Wayne Curve built in 1913. Currently located at NE Bothell Way and 90th Ave NE and a registered historical landmark.

**In the early years** railway or water way was the main route from Seattle to Bothell. Rough dirt logging roads cut through the dense forest were the only other local option for transport. In 1911 the Permanent Highway Act increased federal funds for state highways, and gave standards for grading and surfaces to be used for expanding roads. By 1912 the Pacific Highway was completed and ran from Seattle to Everett, but due to quicksand the portion between Lake Forest Park to Bothell wasn't finished until 1913. This two lane highway was paved with local bricks as an experiment in types of hard surfaces for roads. The completion of the highway was cause for celebration and the entire town of Bothell threw a huge party called "the good road blowout" that included food, dancing and a parade of cars. In 1934 the road was replaced with a four lane highway made of asphalt and only a small portion of the original brick road remains today.

**URBANE VILLAGE  
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